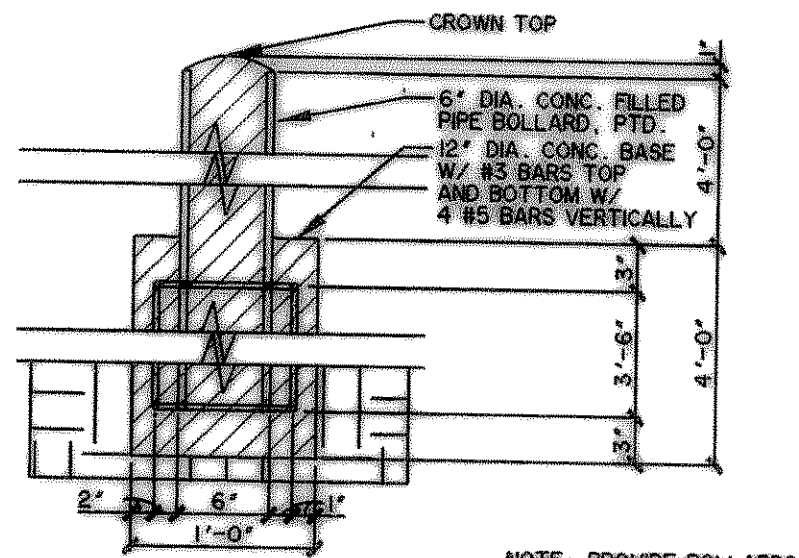
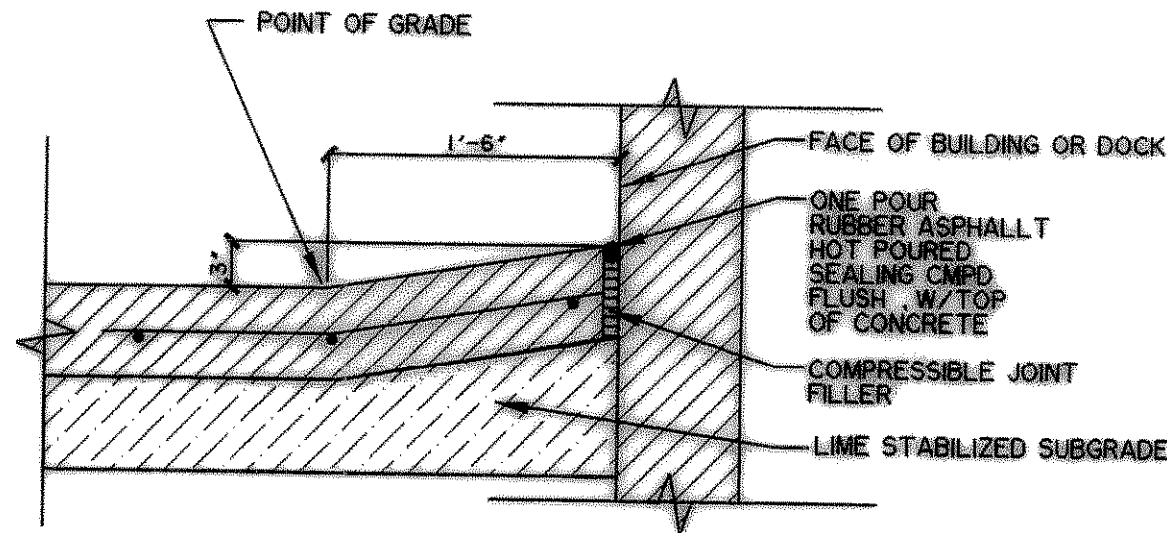


RECORD DRAWING 03/13/07

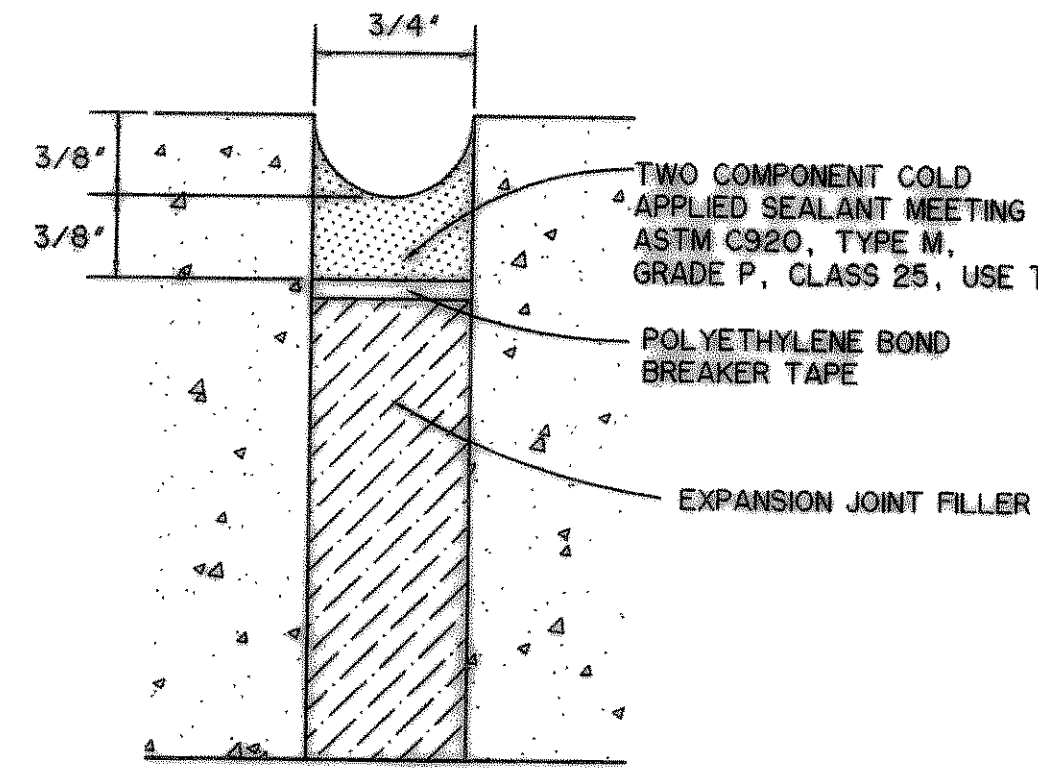


NOTE: PROVIDE BOLLARDS @ ALL EXIT STAIRS LOCATED AT PAVED AREAS AS SHOWN ON PLANS

PIPE BOLLARD
N.T.S.

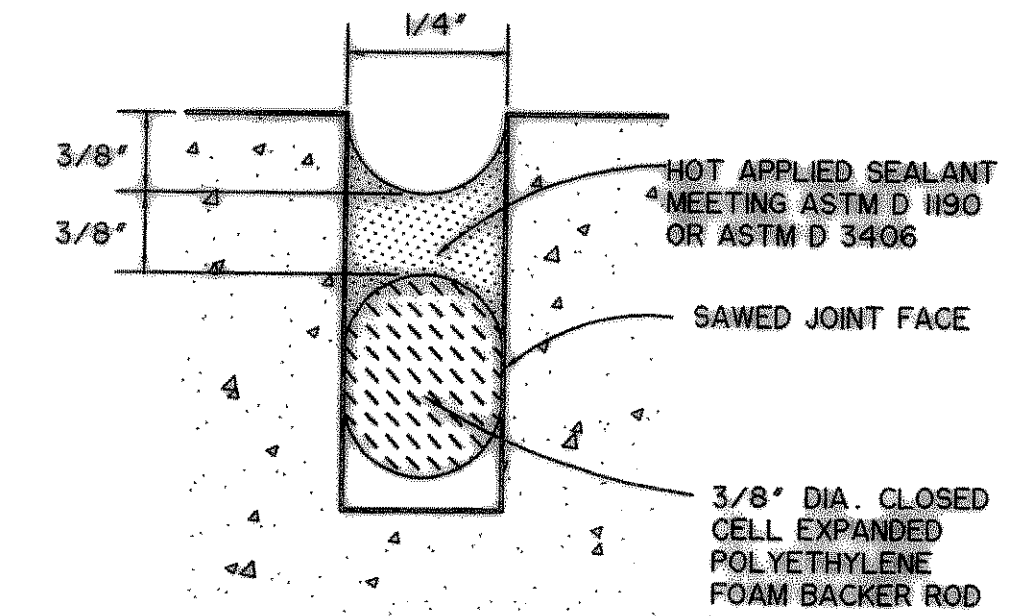


PAVING @ BLDG. FACE OR DOCK
N.T.S.

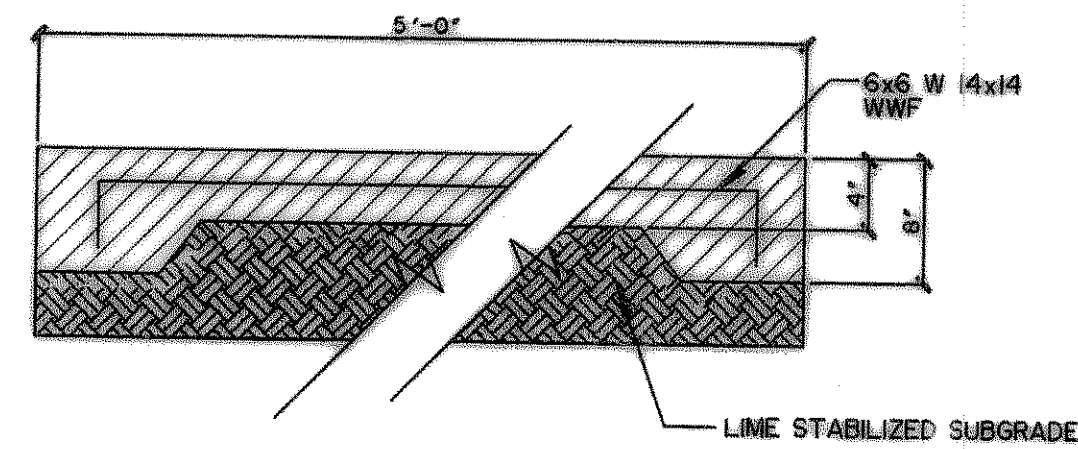


**JOINT SEALANT DETAIL No. 1
SEAL FOR EXPANSION JOINT**
N.T.S.

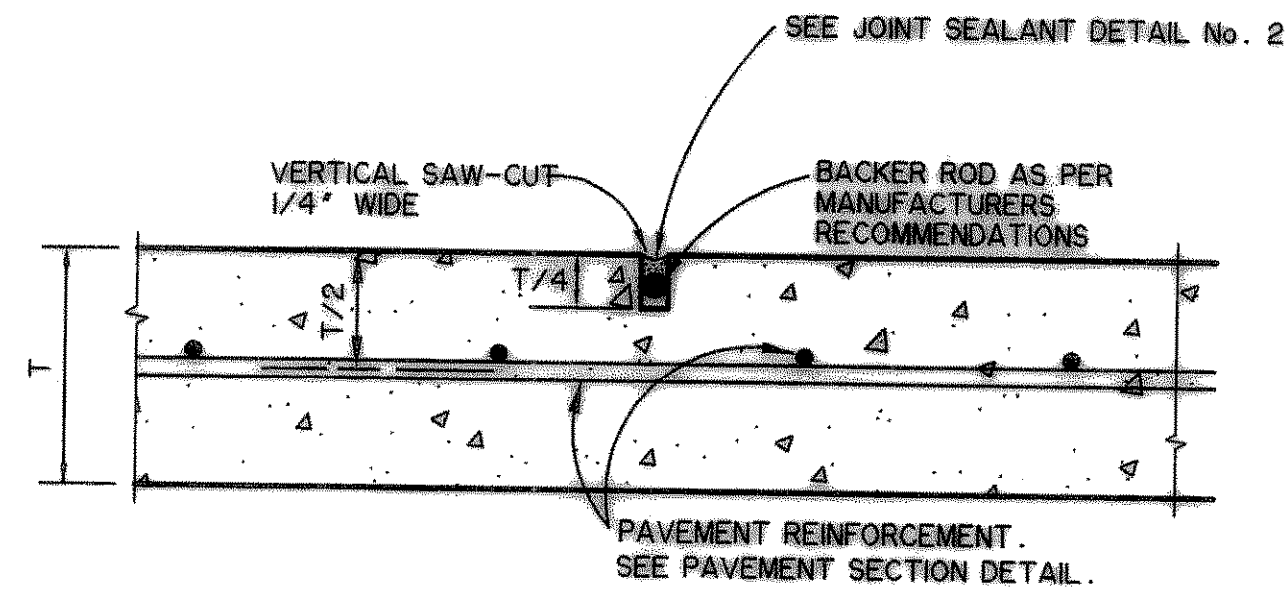
USE CONSTRUCTION JOINT BETWEEN PAVEMENT POURS IF CONCRETE PLACEMENT IS INTERRUPTED OR STOPPED FOR MORE THAN 30 MINUTES.



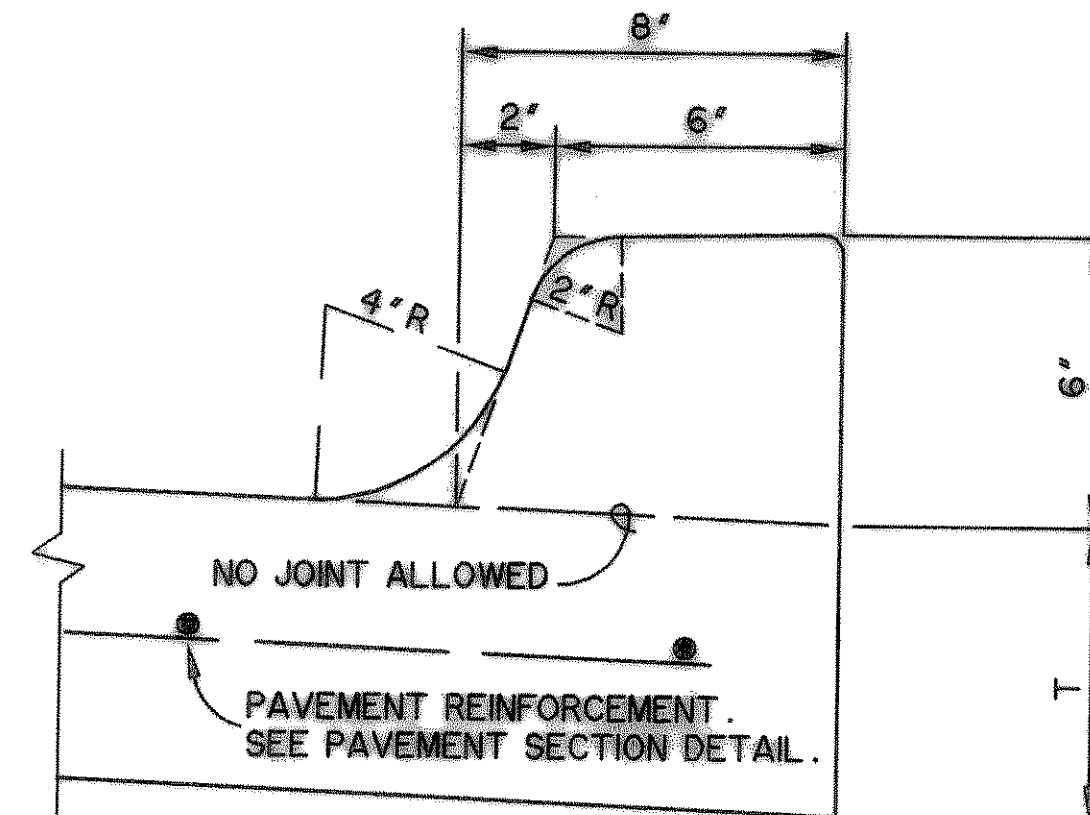
**JOINT SEALANT DETAIL No. 2
SEAL FOR SAWED,
CONSTRUCTION & BUTT JOINT**
N.T.S.



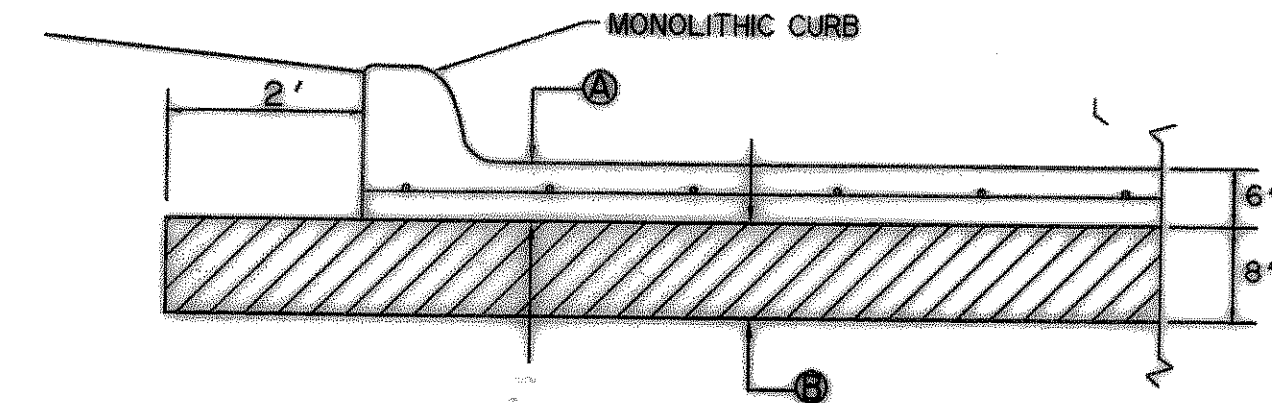
CONCRETE PAD
SCALE: 1"=1'-0"



SAWED CONTRACTION JOINT
N.T.S.

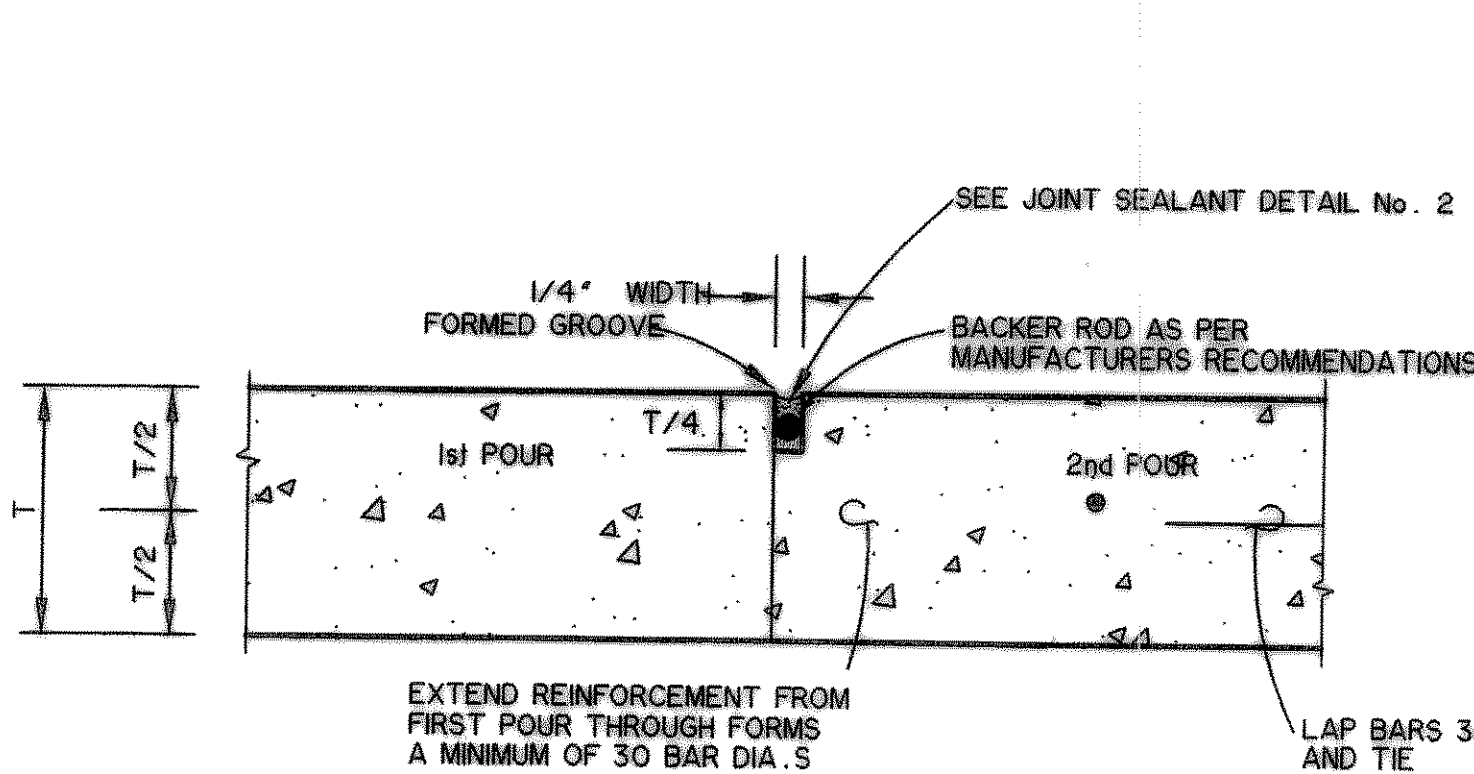


MONOLITHIC CURB DETAIL
N.T.S.

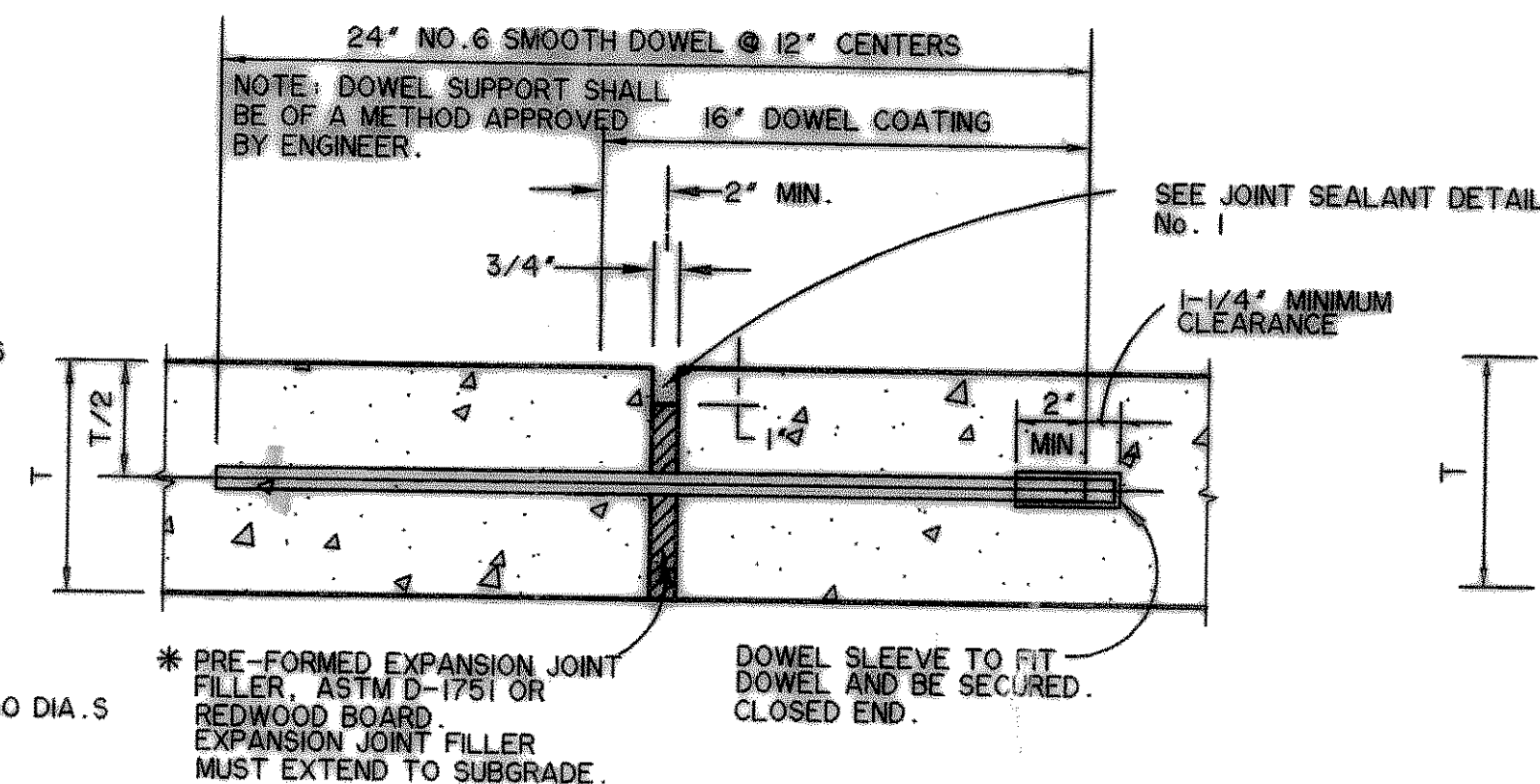


- NOTES: CROSS SECTION PER CITY STANDARDS. SEE GENERAL NOTES.
- ① 6" - 3600 PSI CONC. REINFORCED WITH #3 BARS @ 18" O.C.E.W. PLACED ON CHAIRS. 6 SACK MIX FOR MACHINE PLACEMENT. 6 1/2 SACK MIX FOR HAND PLACEMENT.
 - ② COMPACTED LIME STABILIZED SUBGRADE @ 8% LIME 8" SUBGRADE. COMPACT TO 95% STANDARD PROCTOR DENSITY
 - ③ MATERIAL AND CONSTRUCTION METHODS SHALL CONFORM TO THE APPLICABLE PROVISIONS OF THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION PREPARED BY THE NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS. COARSE AGGREGATE FOR CONCRETE SHALL BE GRADE No. 2 AIR ENTRAINMENT SHALL RANGE FROM 4% TO 6%.
 - DO NOT PLACE SAND OR SELECT FILL BENEATH PAVEMENT FOR LEVEL UP COURSE. UTILIZE ONLY LIME STABILIZED MATERIALS.

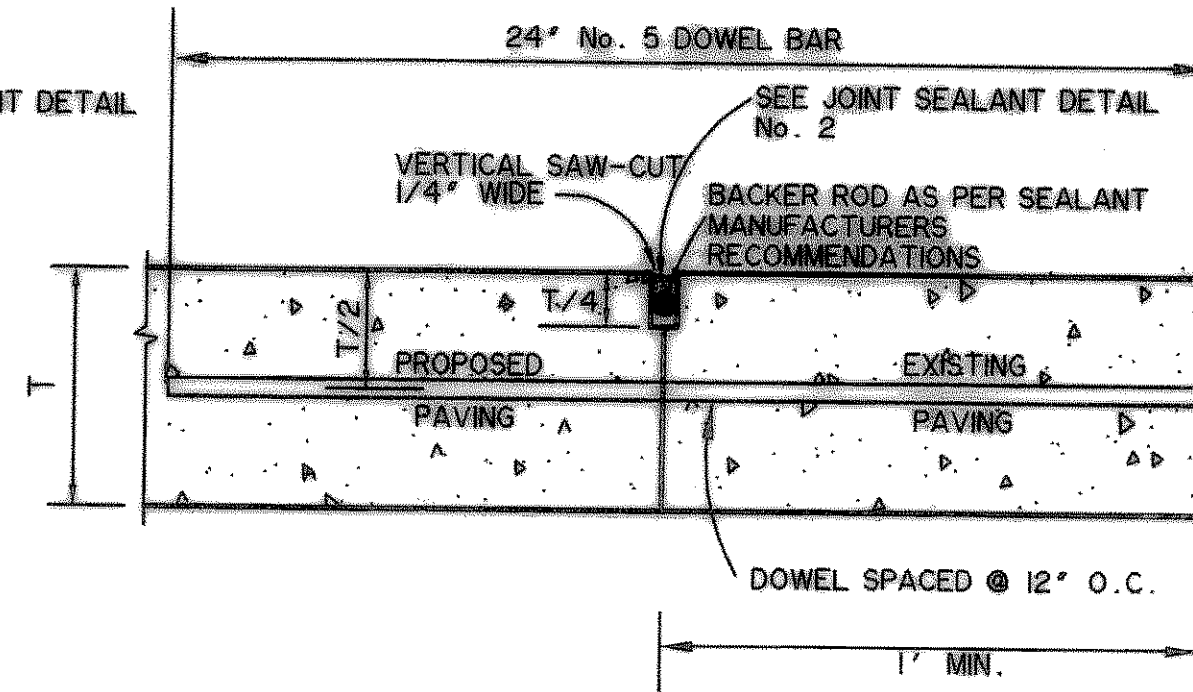
PAVEMENT SECTION-ON SITE
N.T.S.



CONSTRUCTION JOINT
N.T.S.



EXPANSION JOINT
N.T.S.



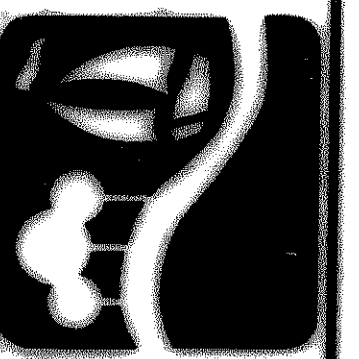
BUTT JOINT
N.T.S.

JOINT NOTES:

T = PAVEMENT THICKNESS

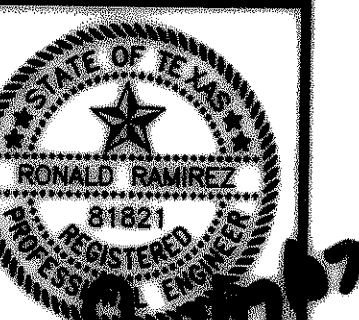
- CONTRACTOR MAY ELECT TO USE DOWELED CURB OR MONOLITHIC CURB
- DOWEL BARS PLACED INTO EXISTING PAVEMENT SHALL BE DRILLED INTO PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL RIG. DRILLING BY HAND IS NOT ACCEPTABLE. PUSHING DOWEL BARS INTO GREEN CONCRETE IS NOT ACCEPTABLE. SECURE DOWEL BARS IN EXISTING PAVING WITH EPOXY GROUT.
- POLYETHYLENE FOAM BACKER ROD DOES NOT SIT ON BOTTOM OF SAW-CUT JOINT. PLACE AT DEPTH INDICATED IN DETAIL.
- IF SEALANT PROTRUDES ABOVE THE SURFACE OF THE PAVEMENT, IT MUST BE REMOVED AND REPLACED.
- SUBMIT MANUFACTURER'S LITERATURE FOR SEALANT, DOCUMENTING PRODUCT COMPLIES WITH ASTM SPECIFICATIONS AND PROVIDING MANUFACTURER'S RECOMMENDATIONS FOR APPLICATION. FOLLOW MANUFACTURER'S RECOMMENDATIONS ON USE OF THE PRODUCT.
- THE CONSTRUCTION JOINT IS TO BE USED BETWEEN SEPARATE POURS OF PROPOSED PAVEMENT. NOTE THAT IT REQUIRES THE REINFORCEMENT TO BE EXTENDED THROUGH THE FORM TO TIE TO THE NEXT POUR. THE BUTT JOINT IS TO BE USED BETWEEN EXISTING CONCRETE PAVEMENT (STREET OR DRIVEWAY) AND PROPOSED PAVEMENT, UNLESS AN EXPANSION JOINT IS CALLED FOR.

PREPARED BY:
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CITY OF
ROCKWALL
WHITMORE'S
EXPANSION

PAVING DETAILS



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