PROJECT:

YOUNG HYUNDAI
PARKING LOT IMPROVEMENTS

1530 EAST INTERSTATE HIGHWAY NO. 30
ROCKWALL, TEXAS

MAY - 2016

"AS-BUILT"

THIS AS-BUILT DRAWING IS BASED ON INFORMATION OBTAINED FROM BUILDING CONTRACTORS DURING CONSTRUCTION AS PROVIDED TO JDJR ENGINEERS & CONSULTANTS, INC.. JDJR ENGINEERS & CONSULTANTS, INC. DOES NOT GUARANTEE THE ACCURACY OR COMPLETENESS OF THESE RECORDS. ALL PROPOSED IMPROVEMENTS WERE CONSTRUCTED EXCEPT FOR THE PARKING LOT ON THE NORTH SIDE OF THE BUILDING. THE EXISTING DRIVE APPROACH FROM THE SERVICE ROAD WAS REMOVED AND THE NEW DRIVE APPROACH CONSTRUCTED.

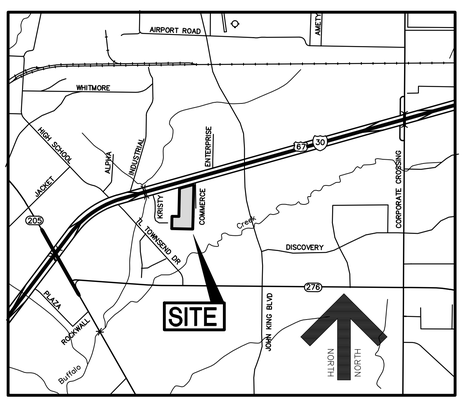
THE POSTED SPEED LIMIT FOR I.H. NO. 30 SERVICE ROAD IS 40 MPH

SHEET INDEX

TXDOT DETAILS

CONCRETE CURB AND CURB AND GUTTER SHEET CCCG–(12)
TRAFFIC CONTROL PLAN STANDARD SHEET TCP(1–5)–12
BARRICADE AND CONSTRUCTION STANDARDS BC(1)–07 THRU BC(12)–07
TEMPORARY EROSION SEDIMENT AND WATER EC(1)–09 POLLUTION CONTROL MEASURES

OWNER:
GORDON ROCKWALL INVESTMENTS, LLC
1551 IH 30 EAST #100
ROCKWALL, TEXAS 75087
PHONE (972) 772-9089 / CELL (214) 507-9831
ATTN: ZACK AMICK
EMAIL: za@tmgconst.com



LOCATION MAP
NOT TO SCALE

STATE OF TEXAS
JAMES DEWEY, JR.
61480
REGISTERED
PROFESSIONAL ENGINEER

THE STANDARD SHEETS, SPECIFICALLY IDENTIFIED IN THIS INDEX OF SHEETS, HAVE BEEN SELECTED BY ME OR UNDER MY RESPONSIBLE SUPERVISION AS BEING APPLICABLE TO THIS PROJECT.

_____ DATE: MAY 18, 2016
JAMES DEWEY, JR., P.E.
REGISTERED PROFESSIONAL ENGINEER NO. 61480

BY SEALING AND SIGNING THESE PERMIT PLANS AS A PROFESSIONAL CIVIL ENGINEER LICENCED TO PRACTICE IN THE STATE OF TEXAS, I CERTIFY THAT THE PROPOSED DRIVEWAY OR PUBLIC STREET CONNECTION(S) TO THE STATE ROADWAY MEETS OR EXCEEDS THE MINIMUM STOPPING SIGHT DISTANCE REQUIRED FOR A DESIGN SPEED OF 60 MILES PER HOUR, BASED ON THE MOST RECENT TXDOT DESIGN MANUAL REQUIREMENTS.

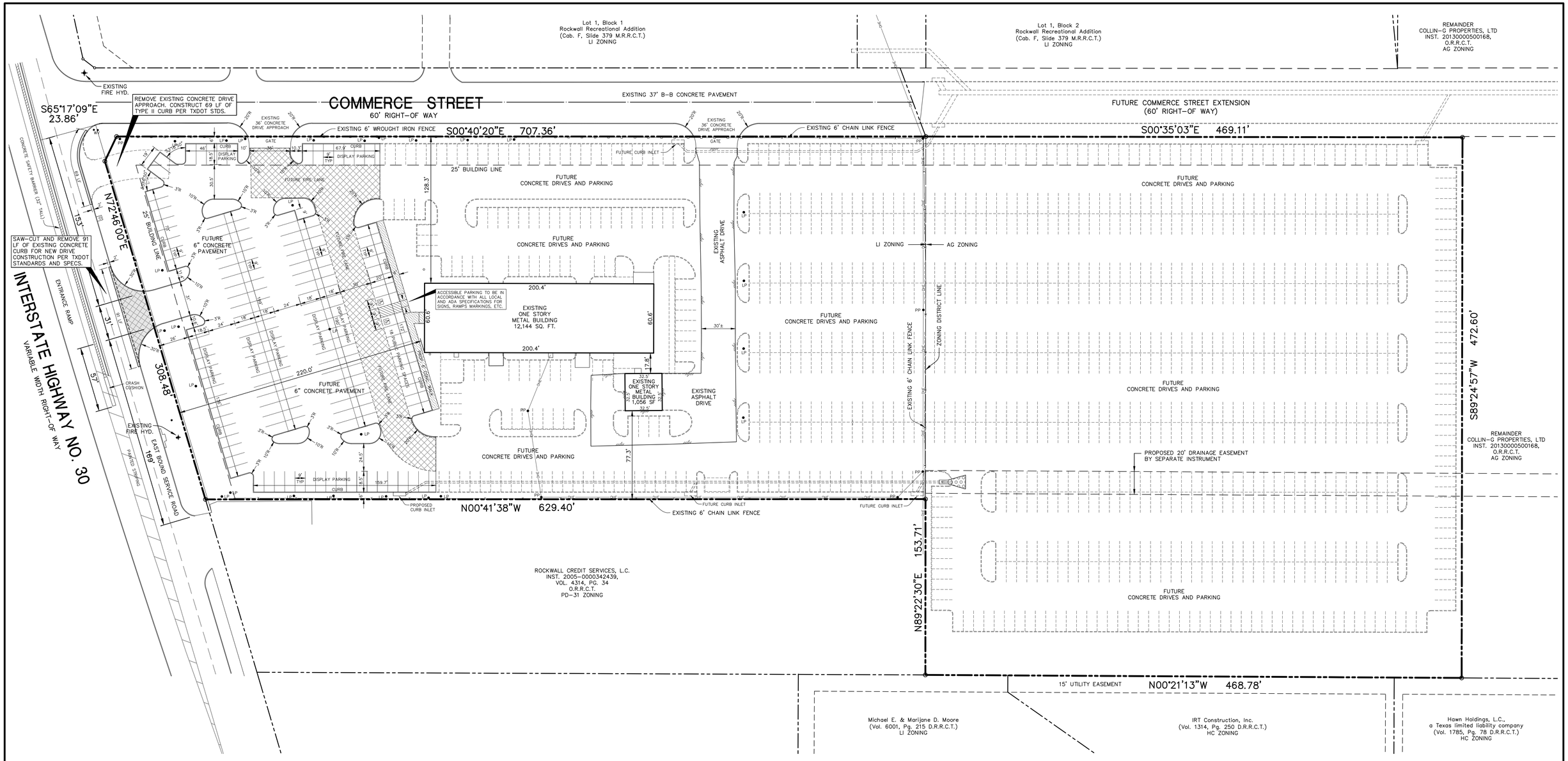
NOTES:

1. ALL CONSTRUCTION WITHIN THE STATE RIGHT OF WAY WILL REQUIRE COMPLIANCE TO TXDOT STANDARD SPECIFICATIONS, STANDARD PLANS, AND TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

2. SPECIFICATIONS ADOPTED BY THE TEXAS DEPARTMENT OF TRANSPORTATION, NOVEMBER 1, 2014, AND SPECIFICATION ITEMS LISTED AND DATED AS FOLLOWS SHALL GOVERN ON THIS PROJECT. FOR ALL WORK WITHIN THE STATE RIGHT OF WAY.

JDJR ENGINEERS AND CONSULTANTS, INC.
TSBPE REGISTRATION NUMBER F-8527
ENGINEERS • SURVEYORS • LAND PLANNERS
2500 Texas Drive Suite 100 Irving, Texas 75062
Tel 972-252-5357 (JDJR) Fax 972-252-8958

JDJR FILE NO. 1115–4–14



NOTE: SEE TXDOT DRIVE APPROACH DETAILS ON SHEET C6A

NORTH

SCALE: 1" = 40'

0' 40' 80' 120'

GRAPHIC SCALE

REVISIONS:	
5/19/15	PER CITY REVIEW
8/07/15	PER CITY REVIEW
2/26/16	RELOCATE DRIVEWAY
3/17/16	PER CITY REVIEW
5/18/16	PER CITY REVIEW

4/05/17 AS-BUILT

STATE OF TEXAS
JAMES DEWEY, JR.
61486
REGISTERED PROFESSIONAL ENGINEER
04/05/17

GENERAL NOTES:

1. ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH CITY OF ROCKWALL STANDARD SPECIFICATIONS, GENERAL DESIGN STANDARDS AND NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS, 3RD EDITION, SPECIFICATIONS (AS AMENDED BY CITY OF ROCKWALL).

2. A PAY ITEM FOR TRENCH SAFETY SYSTEMS THAT MEET REGULATIONS FOR EXCAVATING, TRENCHING, AND SHORING CONTAINED IN SUB PART P, PART 1926 OF THE CODE OF FEDERAL REGULATIONS SHALL BE INCLUDED IN THE OWNER/CONTRACTOR CONTRACT AGREEMENT IN ACCORDANCE WITH H. B. 665.

3. ALL DIMENSIONS SHOWN ARE TO THE BACK OF CURB UNLESS OTHERWISE SHOWN.

4. SEE SHEET C2A & C2B FOR GRADING, PAVING & STORM DRAIN PLANS OF THE SITE.

5. SEE SHEET C5 FOR EROSION CONTROL PLAN OF THE SITE.

6. SEE SHEET C6A & C6B FOR SITE DETAILS.

ACCESSIBILITY NOTES:

1. ALL ACCESSIBLE PARKING AREAS, ROUTES, RAMPS, ETC. SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE TEXAS ACCESSIBILITY STANDARDS (TAS).

2. ALL SIDEWALK RAMPS AND/OR CURB RAMPS SHOWN SHALL HAVE A MAXIMUM VERTICAL RISE OF 6" WITH A MAXIMUM RUNNING SLOPE OF 1:12 (8.33%) AND BE CONSTRUCTED IN ACCORDANCE WITH TAS SECTIONS 4.7 AND 4.8.

3. ALL ACCESSIBLE ROUTES (EXCEPT FOR THE SIDEWALK AND CURB RAMPS) SHALL HAVE A MAXIMUM RUNNING SLOPE OF 1:20 (5%) AND A MAXIMUM CROSS SLOPE OF 1:50 (2%).

4. ALL ACCESSIBLE PARKING SPACES AND ISLES SHALL HAVE A MAXIMUM SLOPE IN ANY DIRECTION OF 1:50 (2%). REFER TO ARCHITECTURAL PLANS FOR DETAILS OF MARKINGS, SIGNS, ETC.

LEGEND

- PROPOSED CONCRETE PAVEMENT (6" 3000 PSI, MINIMUM 5.5 SACK, ON COMPACTED SUBGRADE)
- PROPOSED CONCRETE PAVEMENT (6" 3,600 PSI, MINIMUM 6.5 SACK, ON COMPACTED SUBGRADE)
- PROPOSED CONCRETE WALK (4" 3000 PSI, MINIMUM 5.5 SACK, ON COMPACTED SUBGRADE)

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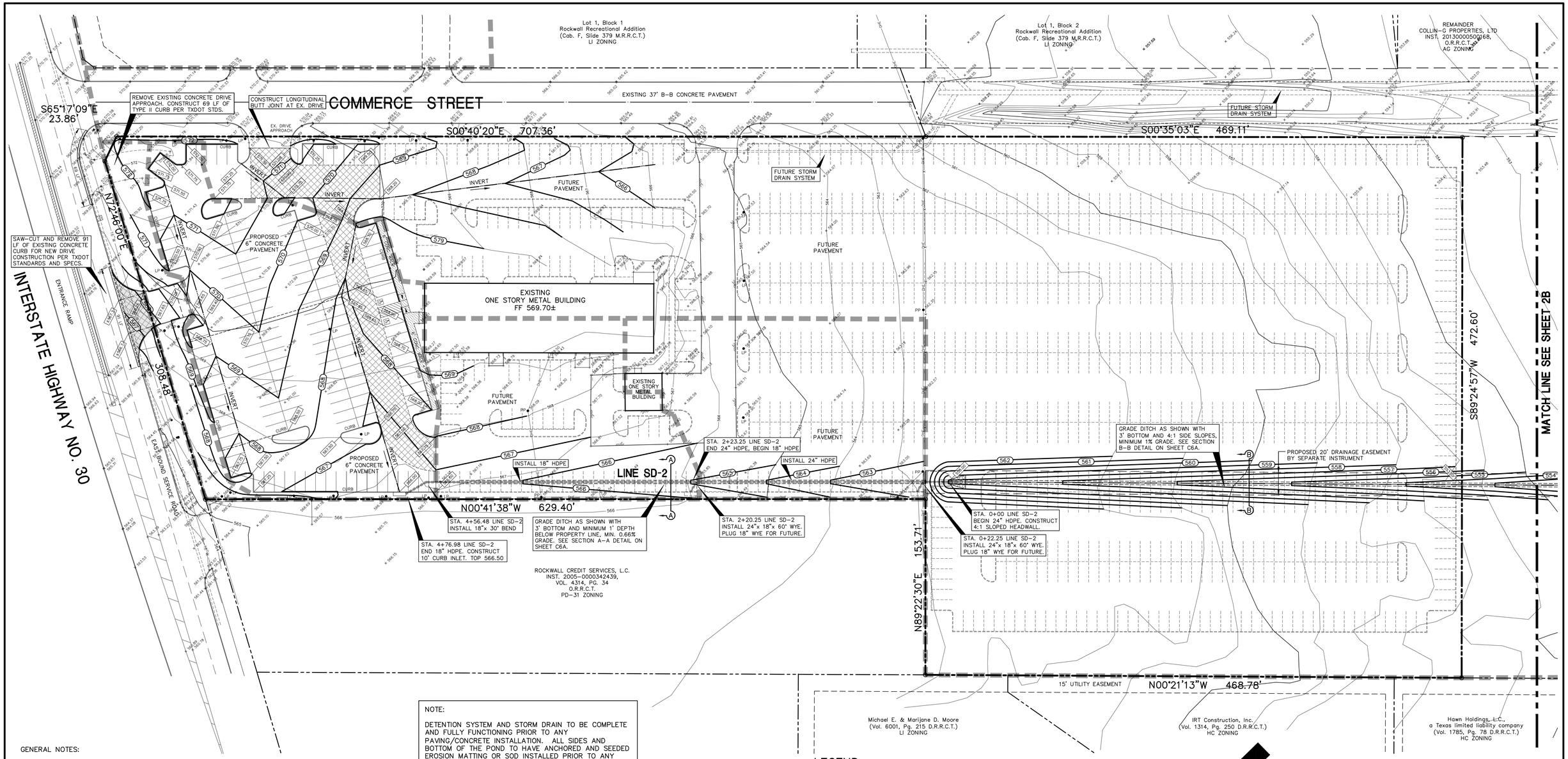
SHEET TITLE:

SITE DIMENSIONAL CONTROL PLAN
YOUNG HYUNDAI
1530 SOUTH INTERSTATE HIGHWAY NO. 30
ROCKWALL, TEXAS

JDJR
PREPARED BY:
ENGINEERS & CONSULTANTS, INC.
TSBPE REGISTRATION NUMBER F-8527
ENGINEERS • SURVEYORS • LAND PLANNERS
2500 Texas Drive Suite 100 Irving, Texas 75062
Tel 972-252-5357 Fax 972-252-8958

DATE: MAR. 25, 2015	DRAWN BY: SAS	SHEET NO.
SCALE: 1" = 40'	CHECKED BY: JDJR	C1 OF 6

JDJR FILE NO. 1115-4-14



GENERAL NOTES:

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2. ALL SPOT ELEVATIONS ADJACENT TO CURBS ARE GUTTER ELEVATIONS UNLESS OTHERWISE SHOWN.

3. ALL SITE PAVING TO BE DONE IN ACCORDANCE TO CITY SPECIFICATIONS AND THE RECOMMENDATIONS AS OUTLINED IN THE SOILS REPORT FOR THIS SITE.

4. ALL SITE GRADING AND SUBGRADE PREPARATION SHALL BE DONE IN ACCORDANCE TO CITY SPECIFICATIONS AND THE RECOMMENDATIONS AS OUTLINED IN THE SOILS REPORT FOR THIS SITE.

5. BARRICADING & TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS IN THE TEXAS MANUAL OF UNIFORM CONTROL DEVICES AND THE CITY OF CARROLLTON REQUIREMENTS.

6. ALL FILL TO BE COMPACTED TO 95% STANDARD DENSITY USING A SHEEPS FOOT ROLLER.

EXISTING UTILITIES NOTES:

1. THE LOCATION OF ALL UNDERGROUND FACILITIES AS INDICATED ON THE PLANS ARE TAKEN FROM PUBLIC RECORDS. JDJR ENGINEERS & CONSULTANTS ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OF SUCH RECORDS AND DOES NOT GUARANTEE THAT ALL UNDERGROUND UTILITIES ARE SHOWN OR ARE LOCATED PRECISELY AS INDICATED.

2. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO MAKE ARRANGEMENTS WITH THE OWNERS OF SUCH UNDERGROUND FACILITIES PRIOR TO WORKING IN THE AREA TO CONFIRM THEIR EXACT LOCATION AND TO DETERMINE WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLANS MAY BE PRESENT.

3. THE CONTRACTOR SHALL PRESERVE AND PROTECT ALL UNDERGROUND FACILITIES FOUND.

4. NOTIFY JDJR ENGINEERS & CONSULTANTS IF ANY UNDERGROUND UTILITIES ARE NOT IN THE LOCATIONS INDICATED ON THESE PLANS (HORIZONTALLY AND VERTICALLY) OR CONFLICT WITH ANY PROPOSED IMPROVEMENTS ASSOCIATED WITH THESE PLANS.

NOTE:

DETENTION SYSTEM AND STORM DRAIN TO BE COMPLETE AND FULLY FUNCTIONING PRIOR TO ANY PAVING/CONCRETE INSTALLATION. ALL SIDES AND BOTTOM OF THE POND TO HAVE ANCHORED AND SEEDED EROSION MATTING OR SOD INSTALLED PRIOR TO ANY CONCRETE INSTALLATION.

NOTE: SEE SHEET C4A & C4B FOR STORM DRAIN AND DITCH PROFILES.

NOTE: SEE TXDOT DRIVE APPROACH DETAILS ON SHEET C6A

ACCESSIBILITY NOTES:

1. ALL ACCESSIBLE PARKING AREAS, ROUTES, RAMPS, ETC. SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE TEXAS ACCESSIBILITY STANDARDS (TAS).

2. ALL SIDEWALK RAMPS AND/OR CURB RAMPS SHOWN SHALL HAVE A MAXIMUM VERTICAL RISE OF 6" WITH A MAXIMUM RUNNING SLOPE OF 1:12 (8.33%) AND BE CONSTRUCTED IN ACCORDANCE WITH TAS SECTIONS 4.7 AND 4.8.

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5. ALL FACILITIES CONSTRUCTED ON STATE RIGHT-OF-WAY SHALL CONFORM TO STATE AND FEDERAL ACCESSIBILITY STANDARDS. APPROVAL AND FINAL INSPECTION BY THE TEXAS DEPARTMENT OF LICENSING AND REGULATION MAY BE NECESSARY. THE GRANTEE IS RESPONSIBLE FOR COORDINATION AND/OR PERMITTING AND WITH THESE ENTITIES. TxDOT MAY REQUIRE EVIDENCE OF COORDINATION.

LEGEND

624	EXISTING CONTOUR LINE
+ 480.25	EXISTING SPOT ELEVATION
704.75	PROPOSED SPOT ELEVATION AT GUTTER (TOP OF CURB IS 6" OR 0.5' HIGHER UNLESS SHOWN OTHERWISE)
618	PROPOSED CONTOUR LINE
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	PROPOSED CONCRETE PAVEMENT (6" 3,600 PSI, MINIMUM 6.5 SACK, ON COMPACTED SUBGRADE)
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	DRAINAGE AREA DIVIDE LINE

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4/05/17 AS-BUILT

STATE OF TEXAS
JAMES DEWEY, JR.
61480
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ENGINEER
04/05/17

BENCH MARK: TXDOT BRASS MONUMENT FOUND AT THE INTERSECTION OF THE EAST LINE OF COMMERCE STREET WITH THE SOUTH LINE OF I.H. 30.

ELEVATION 572.64

SITE T.B.M.: PK NAIL SET IN THE SOUTH LINE OF I.H. 30 25' WEST OF THE NORTHWEST CORNER OF THIS TRACT.

ELEVATION 564.45

SITE T.B.M.: X-CUT SET IN TOP OF CURB INLET NORTHEAST SIDE OF T.L. TOWNSEND DRIVE ±110' NOETH OF BUFFALO CREEK

ELEVATION 534.69

SHEET TITLE:

GRADING, PAVING & STORMDRAIN PLAN NORTH

YOUNG HYUNDAI

1530 SOUTH INTERSTATE HIGHWAY NO. 30

ROCKWALL, TEXAS

JDJR

PREPARED BY:

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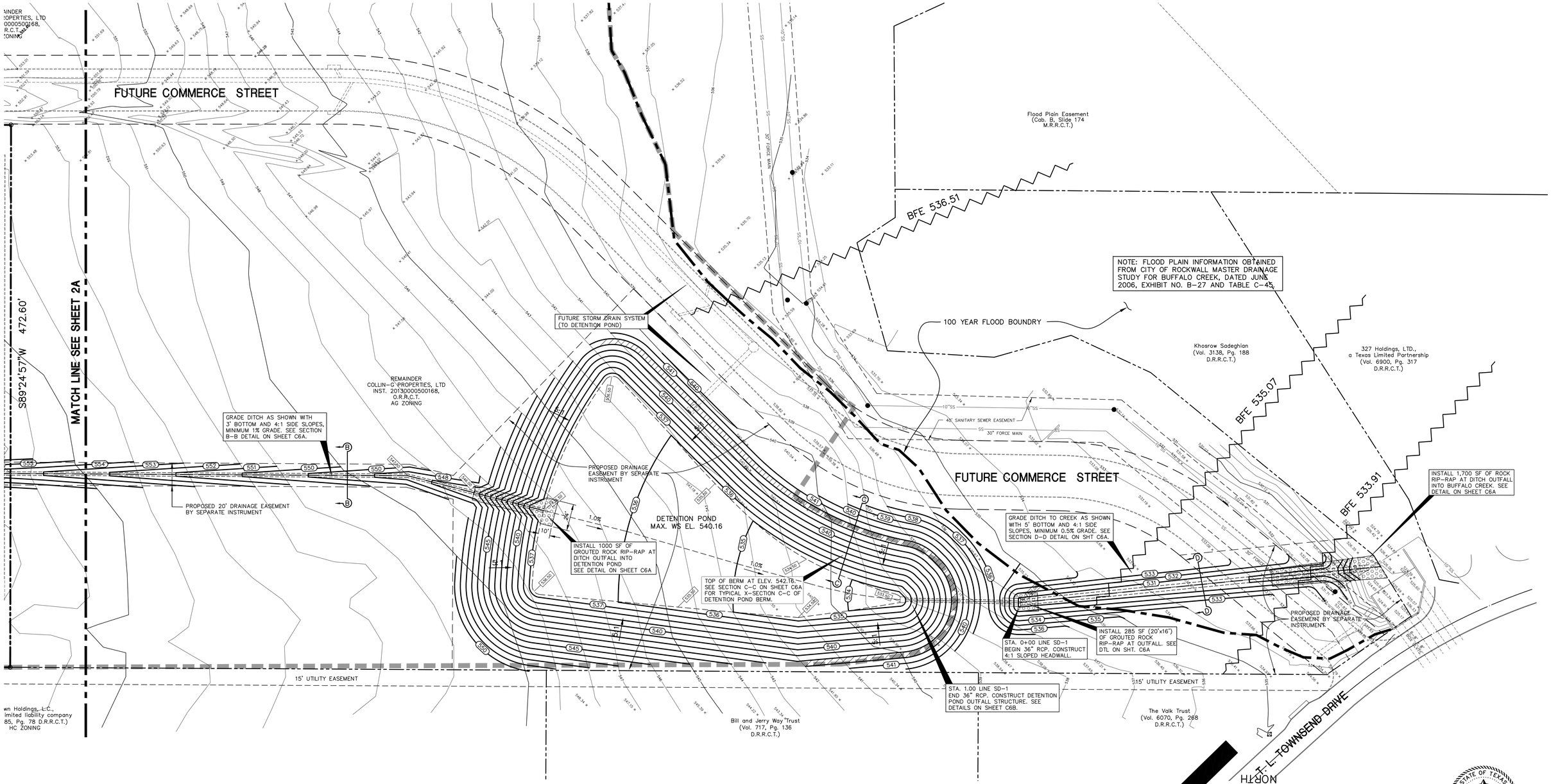

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	INVERT OR FLOWLINE
	PROPOSED GROUTED ROCK RIP-RAP (SEE DETAIL ON SHEET C6A)
	TOP OF DETENTION BOND BERM
	DRAINAGE AREA DIVIDE LINE
	100 YEAR FLOOD BOUNDARY LINE

"AS-BUILT"

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4/05/17	AS-BUILT

STATE OF TEXAS
JAMES DEWEY, JR.
61458
REGISTERED PROFESSIONAL ENGINEER
04/05/17

SHEET TITLE:

GRADING & STORM DRAIN PLAN SOUTH (DETENTION POND)
YOUNG HYUNDAI
1530 SOUTH INTERSTATE HIGHWAY NO. 30
ROCKWALL, TEXAS

JDJR
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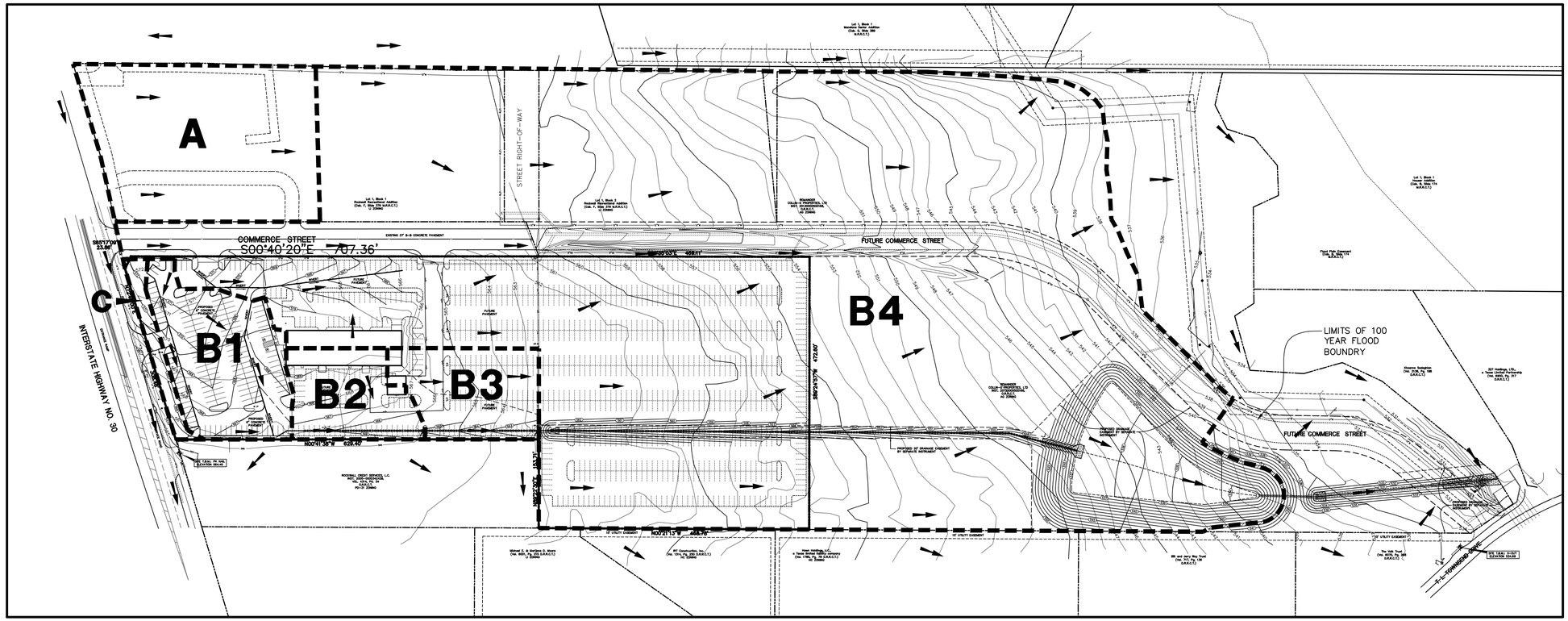

NORTH

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0' 100' 200' 300'

GRAPHIC SCALE

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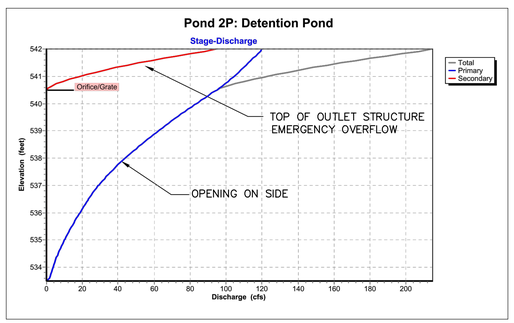
DRAINAGE AREA MAP

SUMMARY OF DETENTION CALCULATIONS

STORM DURATION MINUTES	5-YEAR STORM			10-YEAR STORM			25-YEAR STORM			100-YEAR STORM		
	PEAK INFLOW CFS	PEAK OUTFLOW CFS	MAX. PONDING ELEV. FT (MSL)/FEET	PEAK INFLOW CFS	PEAK OUTFLOW CFS	MAX. PONDING ELEV. FT (MSL)/FEET	PEAK INFLOW CFS	PEAK OUTFLOW CFS	MAX. PONDING ELEV. FT (MSL)/FEET	PEAK INFLOW CFS	PEAK OUTFLOW CFS	MAX. PONDING ELEV. FT (MSL)/FEET
20	134.29	50.50	538.37/4.87	161.70	60.36	538.89/5.39	180.88	67.34	539.24/5.74	227.47	84.40	540.04/6.54
30	112.37	56.74	538.71/5.21	131.55	66.27	539.19/5.79	150.74	75.86	539.65/6.15	189.10	95.18	540.51/7.01
40	93.18	56.68	538.70/5.20	109.63	66.64	539.21/5.81	126.07	76.63	539.69/6.19	158.96	97.62	540.58/7.08
50	76.74	52.96	538.51/5.01	95.92	66.23	539.19/5.69	109.63	75.74	539.65/6.15	137.03	94.76	540.50/7.00
60	71.26	53.78	538.55/5.05	82.22	62.12	538.98/5.48	95.92	72.57	539.50/6.00	123.33	93.46	540.44/6.94

NOTE: THE SHADED LINE REPRESENTS THE STORM DURATION THAT MAXIMIZES THE REQUIRED DETENTION VOLUME AND OUTFLOW RATES.

MAXIMUM OUTFLOW ALLOWED (PRE-DEVELOPMENT CONDITIONS) = 58.26 CFS 5-YEAR STORM
MAXIMUM OUTFLOW ALLOWED (PRE-DEVELOPMENT CONDITIONS) = 70.15 CFS 10-YEAR STORM
MAXIMUM OUTFLOW ALLOWED (PRE-DEVELOPMENT CONDITIONS) = 78.48 CFS 25-YEAR STORM
MAXIMUM OUTFLOW ALLOWED (PRE-DEVELOPMENT CONDITIONS) = 98.69 CFS 100-YEAR STORM



STAGE – DISCHARGE CURVES

Asymmetrical Weir

An asymmetrical weir can be used to model an arbitrary weir crest, such as water spilling over a roadway.

To calculate the flow through an asymmetrical weir, the weir is divided into a number of rectangular and half-vee sections. The total flow is determined by adding the flow through each section, according to the trapezoidal weir equation using the rise of each section. For the half-vee sections, the flow is one-half the flow for a corresponding full-vee.

If tailwater is present, the discharge is the sum of two parts:

1) Trapezoidal weir flow for the portion of the opening that lies above the tailwater, with the head and height measured from the tailwater elevation, rather than from the weir invert

2) Constant-head orifice flow for the portion of the opening that lies below the tailwater

Note: An asymmetrical weir will give exactly the same result as using several separate weirs to describe the entire weir opening.

Sharp-Crested Rectangular Weir

The free discharge of a sharp-crested rectangular weir is determined by the following equation from Open Channel Hydraulics by Chow:

$Q = C\ Le\ H^{1.5}$

Where:

C=Weir coefficient

Le=Effective weir length

H=Head (above invert elevation)

The effective weir length (Le) is reduced by the presence of end contractions according to the following equation:

$Le = L - \frac{0.1nH}{10}$ (but never < L/2)

where:

L=Actual crest length

n=Number of end contractions (0,1 or 2)

In practice, the weir coefficient C may vary slightly based on the crest height and the resulting turbulence. If the crest height is specified, the English weir coefficient is determined by the equation:

$C = 3.27 + 0.4\frac{H}{P}$

where:

P=Height of the crest above the approach channel.

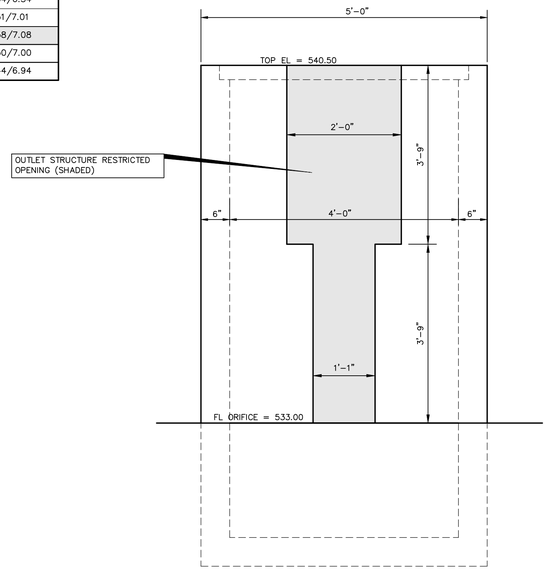
If P is not specified (left blank) a fixed coefficient of 3.27 is used without adjustment.

If the weir rise is specified, and the head exceeds the rise, orifice flow exists and the discharge is given by:

$Q = C\ Le\ [\ H^{1.5} - (H-M)^{1.5}]$

where:

M=Rise



OUTFALL ORIFICE DETAIL (NORTHWEST SIDE OF BOX)

PRE DEVELOPMENT DRAINAGE DATA (FOR DETENTION POND DESIGN)

AREA NO.	ACRES	C	T_C MIN	I_5 IN/HR	Q_5 CFS	I_{10} IN/HR	Q_{10} CFS	I_{25} IN/HR	Q_{25} CFS	I_{100} IN/HR	Q_{100} CFS	COMMENTS
A	2.40	0.90	20	4.9	10.58	5.9	12.74	6.6	14.26	8.3	17.93	EXISTING DEVELOPED LOT, FUTURE FLOW TO DETENTION POND
B1-B4	27.80	0.35	20	4.9	47.68	5.9	57.41	6.6	64.22	8.3	80.76	ULTIMATE FLOW TO DETENTION POND
TOTAL TO POND	30.20				58.26		70.15		78.48		98.69	ALLOWABLE FLOW FROM POND (EXISTING FLOW)
C	0.20	0.35	10	6.1	0.43	7.1	0.50	8.3	0.58	9.8	0.69	LANDSCAPE FRONT YARD TO I.H. 30 BY-PASS DETENTION POND

POST DEVELOPMENT DRAINAGE DATA (FOR DETENTION POND DESIGN)

AREA NO.	ACRES	C	T_C MIN	I_5 IN/HR	Q_5 CFS	I_{10} IN/HR	Q_{10} CFS	I_{25} IN/HR	Q_{25} CFS	I_{100} IN/HR	Q_{100} CFS	COMMENTS
A	2.40	0.90	10	6.1	13.18	7.1	15.34	8.3	17.93	9.8	21.17	EXISTING DEVELOPED LOT, FUTURE FLOW TO DETENTION POND
B1	1.04	0.90	10	6.1	5.71	7.1	6.65	8.3	7.77	9.8	9.17	PROPOSED DEVELOPEMENT, FLOW TO DETENTION POND
B2	0.72	0.90	10	6.1	3.95	7.1	4.60	8.3	5.38	9.8	6.35	PROPOSED DEVELOPEMENT, FLOW TO DETENTION POND
B3	0.84	0.90	10	6.1	4.61	7.1	5.37	8.3	6.27	9.8	7.41	PROPOSED DEVELOPEMENT, FLOW TO DETENTION POND
B4	25.20	0.90	10	6.1	138.35	7.1	161.03	8.3	188.24	9.8	222.26	FUTURE DEVELOPEMENT, FUTURE FLOW TO DETENTION POND
TOTAL TO POND	30.20				165.80		192.99		225.59		258.96	FUTURE DEVELOPEMENT, TOTAL PEAK FLOWS TO POND
C	0.20	0.90	10	6.1	1.10	7.1	1.28	8.3	1.49	9.8	1.76	LANDSCAPE FRONT YARD TO I.H. 30 BY-PASS DETENTION POND

POND VOLUME SUMMARY

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)
533.50	0	0	0
534.00	918	230	230
535.00	7,484	4,201	4,431
536.00	21,399	14,442	18,872
536.50	36,545	14,486	33,358
537.00	38,804	18,837	52,195
538.00	43,440	41,122	93,317
539.00	48,233	45,837	139,154
540.00	53,183	50,708	189,862
541.00	58,290	55,737	245,598
542.00	63,555	60,923	306,521

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ELEVATION 572.64

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REVISIONS:	
5/19/15	PER CITY REVIEW
8/07/15	PER CITY REVIEW
2/26/16	RELOCATE DRIVEWAY
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STATE OF TEXAS
JAMES DEWEY JR.
61480
REGISTERED
ENGINEER
04/05/17

SHEET TITLE:
DRAINAGE AREA MAP (ULTIMATE DEVELOPMENT) AND DETENTION CALCULTATIONS
YOUNG HYUNDAI
1530 SOUTH INTERSTATE HIGHWAY NO. 30
ROCKWALL, TEXAS

PREPARED BY:
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SCALE:	1" = 100'	CHECKED BY:	JDJR	C3A OF 6

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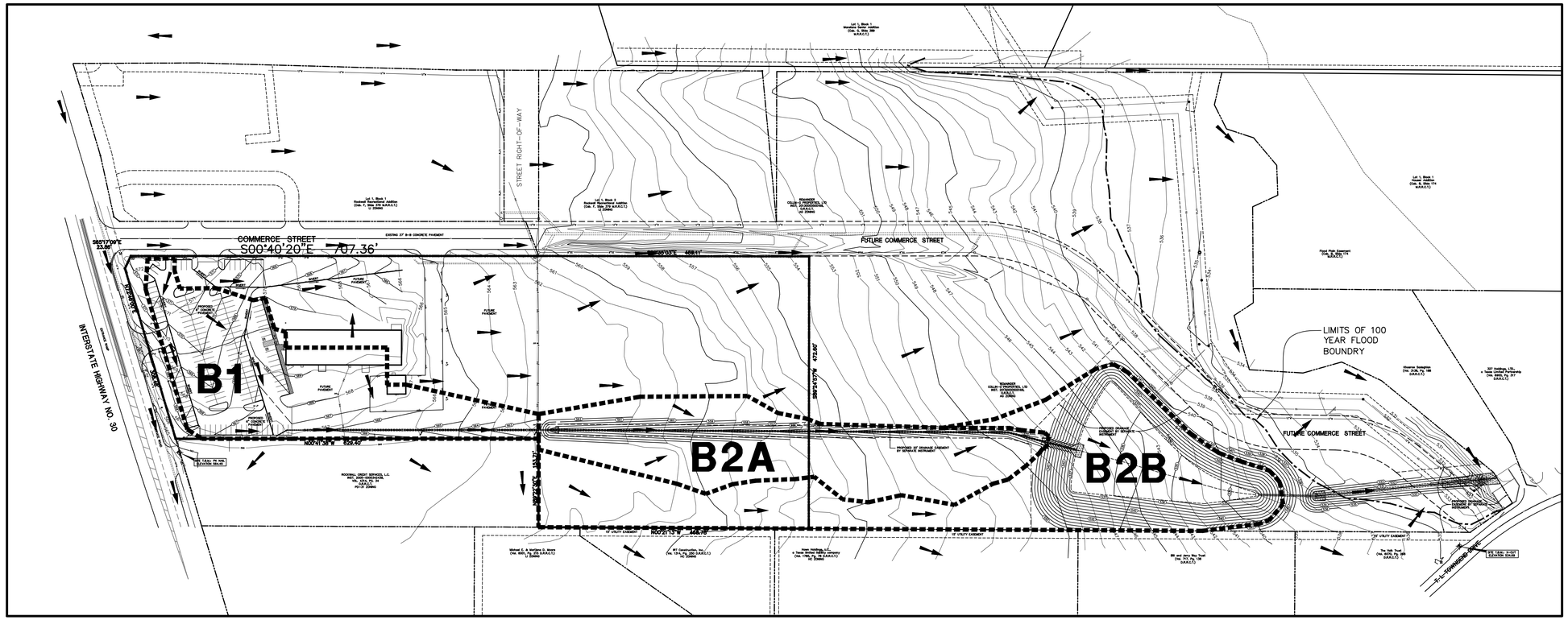

NORTH

SCALE: 1" = 100'

0' 100' 200' 300'

GRAPHIC SCALE

"AS-BUILT"

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DRAINAGE AREA MAP

SUMMARY OF DETENTION CALCULATIONS

STORM DURATION MINUTES	5-YEAR STORM			10-YEAR STORM			25-YEAR STORM			100-YEAR STORM		
	PEAK INFLOW CFS	PEAK OUTFLOW CFS	MAX. PONDING ELEV. FT (MSL)/FEET	PEAK INFLOW CFS	PEAK OUTFLOW CFS	MAX. PONDING ELEV. FT (MSL)/FEET	PEAK INFLOW CFS	PEAK OUTFLOW CFS	MAX. PONDING ELEV. FT (MSL)/FEET	PEAK INFLOW CFS	PEAK OUTFLOW CFS	MAX. PONDING ELEV. FT (MSL)/FEET
10	24.84	11.93	535.24/1.74	28.91	13.05	535.38/1.88	33.80	14.28	535.52/2.02	39.90	15.70	535.69/2.19
20	20.31	13.66	535.45/1.95	24.45	15.36	535.65/2.15	27.35	16.48	535.77/2.27	34.40	19.00	536.05/2.55
30	16.99	13.68	535.45/1.95	19.89	15.27	535.64/2.14	22.79	16.76	535.81/2.31	28.60	19.49	536.10/2.60
40	14.09	12.68	535.33/1.83	16.58	14.36	535.53/2.03	19.06	15.94	535.71/2.21	24.04	18.87	536.04/2.54
50	11.60	11.14	535.15/1.65	14.50	13.45	535.42/1.92	16.58	14.98	535.60/2.10	20.72	17.84	535.93/2.43

NOTE: THE SHADED LINE REPRESENTS THE STORM DURATION THAT MAXIMIZES THE REQUIRED DETENTION VOLUME AND OUTFLOW RATES.

MAXIMUM OUTFLOW ALLOWED (PRE-DEVELOPMENT CONDITIONS) = 14.11 CFS 5-YEAR STORM
MAXIMUM OUTFLOW ALLOWED (PRE-DEVELOPMENT CONDITIONS) = 16.97 CFS 10-YEAR STORM
MAXIMUM OUTFLOW ALLOWED (PRE-DEVELOPMENT CONDITIONS) = 18.99 CFS 25-YEAR STORM
MAXIMUM OUTFLOW ALLOWED (PRE-DEVELOPMENT CONDITIONS) = 23.87 CFS 100-YEAR STORM

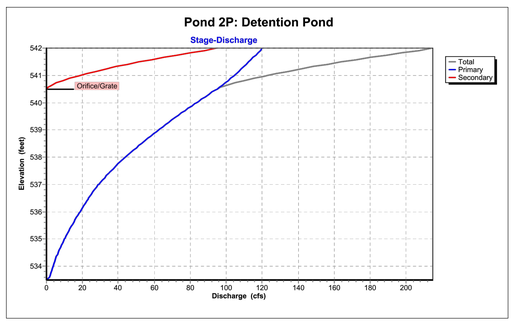
THE SAME RESTRICTED OPENING THAT PROVIDES THE DETENTION FOR THE ULTIMATE DEVELOPMENT ALSO PROVIDES ADEQUATE RESTRICTION (DETENTION) FOR THE INTERIM CONDITIONS.

INTERIM PRE DEVELOPMENT DRAINAGE DATA (FOR DETENTION POND DESIGN)

AREA NO.	ACRES	C	T_C MIN	I_5 IN/HR	Q_5 CFS	I_{10} IN/HR	Q_{10} CFS	I_{25} IN/HR	Q_{25} CFS	I_{100} IN/HR	Q_{100} CFS	COMMENTS
B1	2.28	0.35	20	4.9	3.91	5.9	4.71	6.60	5.27	8.3	6.62	EXISTING DEVELOPED LOT, FUTURE FLOW TO DETENTION POND
B2A	2.38	0.35	20	4.9	4.08	5.9	4.91	6.60	5.50	8.3	6.91	INTERIM FLOW TO DETENTION POND (IN DITCH B-B)
B2B	3.56	0.35	20	4.9	6.12	5.9	7.35	6.60	8.22	8.3	10.34	INTERIM FLOW TO DETENTION POND
TOTAL TO POND	8.22				14.11		16.97		18.99		23.87	ALLOWABLE FLOW FROM POND (EXISTING FLOW)

INTERIM POST DEVELOPMENT DRAINAGE DATA (FOR DETENTION POND DESIGN)

AREA NO.	ACRES	C	T_C MIN	I_5 IN/HR	Q_5 CFS	I_{10} IN/HR	Q_{10} CFS	I_{25} IN/HR	Q_{25} CFS	I_{100} IN/HR	Q_{100} CFS	COMMENTS
B1	2.28	0.90	10	6.1	12.52	7.1	14.57	8.3	17.03	9.8	20.11	PROPOSED DEVELOPEMENT, FLOW TO DETENTION POND
B2A	2.38	0.35	10	6.1	5.08	7.1	5.91	8.3	6.91	9.8	8.16	INTERIM UNDEVELOPED FLOW TO DETENTION POND (IN DITCH B-B)
B2B	3.56	0.35	10	6.1	7.60	7.1	8.85	8.3	10.34	9.8	12.21	INTERIM FLOW TO DETENTION POND
TOTAL TO POND	8.22				25.20		29.33		34.28		40.48	



STAGE – DISCHARGE CURVES

Asymmetrical Weir

An asymmetrical weir can be used to model an arbitrary weir crest, such as water spilling over a roadway.

To calculate the flow through an asymmetrical weir, the weir is divided into a number of rectangular and half-vee sections. The total flow is determined by adding the flow through each section, according to the trapezoidal weir equation using the rise of each section. For the half-vee sections, the flow is one-half the flow for a corresponding full-vee.

If tailwater is present, the discharge is the sum of two parts:

1) Trapezoidal weir flow for the portion of the opening that lies above the tailwater, with the head and height measured from the tailwater elevation, rather than from the weir invert

2) Constant-head orifice flow for the portion of the opening that lies below the tailwater

Note: An asymmetrical weir will give exactly the same result as using several separate weirs to describe the entire weir opening.

Sharp-Crested Rectangular Weir

The free discharge of a sharp-crested rectangular weir is determined by the following equation from Open Channel Hydraulics by Chow:

$Q = C\ Le\ H^{1.5}$

Where:

C=Weir coefficient

Le=Effective weir length

H=Head (above invert elevation)

The effective weir length (Le) is reduced by the presence of end contractions according to the following equation:

$Le = L - \frac{n\,H}{10}$ (but never < L/2)

where:

L=Actual crest length

n=Number of end contractions (0,1 or 2)

In practice, the weir coefficient C may vary slightly based on the crest height and the resulting turbulence. If the crest height is specified, the English weir coefficient is determined by the equation:

$C = 3.27 + 0.4\ \frac{H}{P}$

where:

P=Height of the crest above the approach channel.

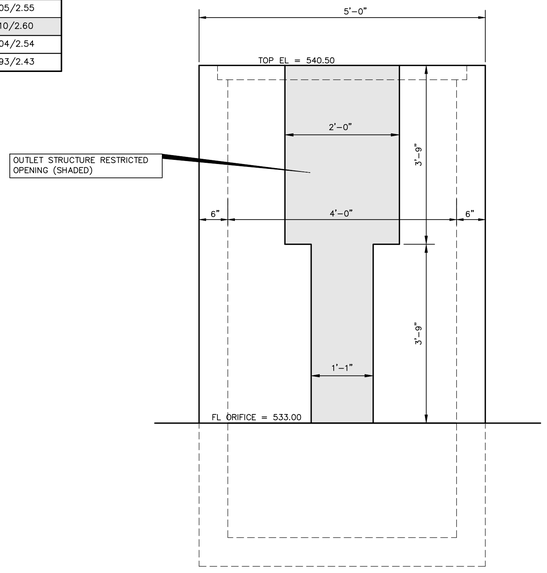
If P is not specified (left blank) a fixed coefficient of 3.27 is used without adjustment.

If the weir rise is specified, and the head exceeds the rise, orifice flow exists and the discharge is given by:

$Q = C\ Le\ [\ H^{1.5} - (H-M)^{1.5}\ ]$

where:

M=Rise



OUTFALL ORIFICE DETAIL (NORTHWEST SIDE OF BOX)

POND VOLUME SUMMARY

Elevation (feet)	Surf.Area (sq-ft)	Inc.Store (cubic-feet)	Cum.Store (cubic-feet)
533.50	0	0	0
534.00	918	230	230
535.00	7,484	4,201	4,431
536.00	21,399	14,442	18,872
536.50	36,545	14,486	33,358
537.00	38,804	18,837	52,195
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STATE OF TEXAS
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61458
LICENSED PROFESSIONAL ENGINEER
04/05/17

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ROCKWALL, TEXAS

PREPARED BY:
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TSBPE REGISTRATION NUMBER F-8527
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Tel 972-252-5357 Fax 972-252-8958

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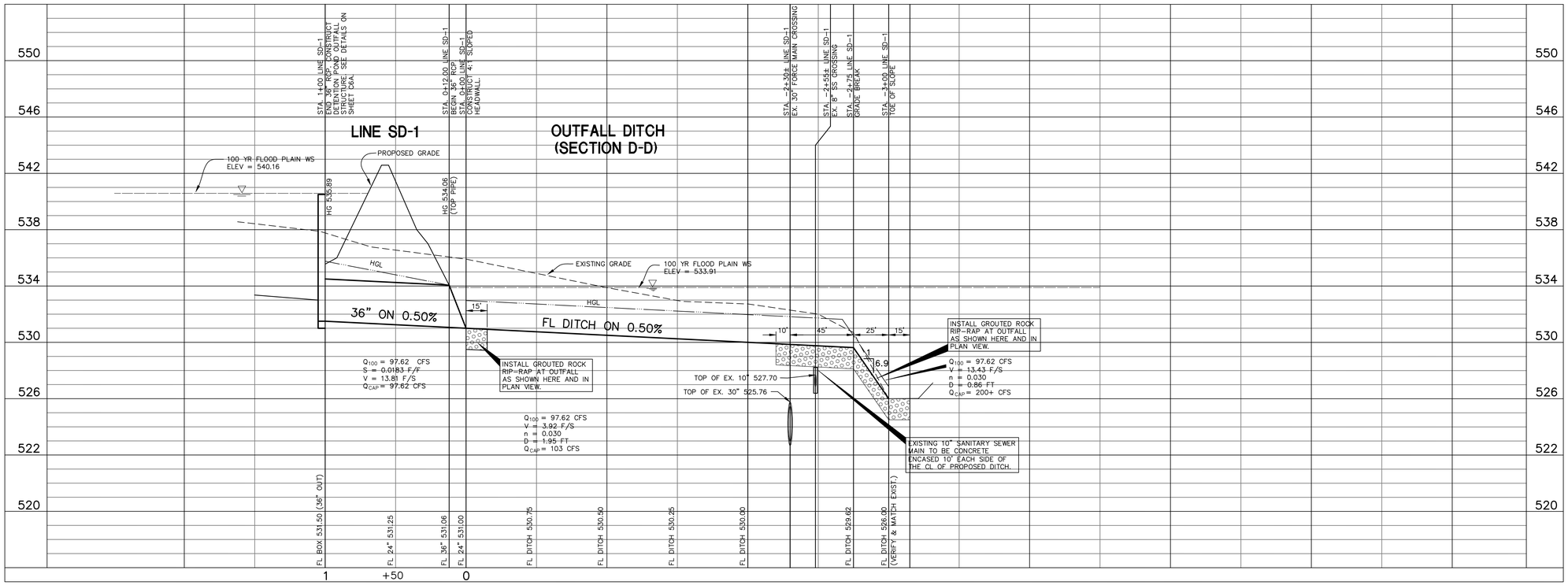
INLET DESIGN CALCULATIONS

BY Jim Dewey, Jr.

INLET		Design Storm Freq. (yrs.)	AREA RUNOFF Q = CIA					Carry-over from upstream inlet (c.f.s.)	Total gutter or invert flow (c.f.s.)	Gutter or invert Capacity (c.f.s.)	Gutter or invert Slope (ft./100 ft.)	Crown Type	SELECTED INLET					Carry-over to downstream inlet (c.f.s.)
No.	Location		Time of Conc. (Min.)	Intensity I (in./hr.)	Runoff Coeff. "C"	Area (Acres)	"Q" (c.f.s.)						Length "L" (ft.)	Area of opening (sq. ft.)	Head on grate (in.)	Type	CAP. (CFS)	
1	4+76.98 SD-2	100	10	9.80	0.9	1.04	9.17	0.00	9.17	NA	1.00	INVERT	10			CURB	>15	1.04

COMPUTATION SHEET
HYDRAULIC COMPUTATIONS FOR STORM DRAINS
STORM DRAIN HYDRAULIC CALCULATIONS TABLE

FROM	TO	Pipe Length	Drainage Area Incremental No.	Drainage Area Incremental Area	Drainage Area Total Area	Runoff "c"	Incr. cA	Total cA	Time of Concentration Inlet	Time of Concentration Travel	Time of Concentration Total	5-year Intensity	100-year Intensity	Q-5 Runoff	A100 Runoff	Inlet Bypass	Q pipe	Pipe Size	n	Sf	HGL D/S Elev.	HGL U/S Elev.	HEAD LOSS CALCULATIONS V1 (in)	V2 (out)	$V1^2/2G$	$V2^2/2G$	Kj	$KjV1^2/2G$	Hk	Design HGL	Invert Elev. FROM	Invert Elev. TO	T/C ELEV.	COMMENTS
		feet							min.	min.	min.	in/hr	in/hr	cfs	cfs	cfs	cfs	in.		ft/ft	Elev.	Elev.	ft/sec	ft/sec	ft.	ft.		ft.	ft.	Elev.	ft.	ft.	ft.	
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	
SD-1																															513.50	513.81	±520	
0+12	1+00	88	OUTFLOW FROM DETENTION POND											→	99.86		99.86	36	0.012	0.0191	534.06	535.74	14.13									531.06	531.50	±540	
SD-2																																		
0+00	0+22	22	B3	0.84	~~2.88~~	0.90	0.76	2.34			10		9.80		22.93		22.93	24	0.012	0.0088	560.00	560.19	7.30	4.94					0.54	560.00	558.00	558.19	±562	
0+22	2+23	201	B2	0.72	1.76	0.90	0.65	1.58			10		9.80		15.52		15.52	24	0.012	0.0040	560.73	561.53	4.94	PARTIAL FLOW (HGL INSIDE PIPE)							558.19	559.87	±563	
2+23	4+56	233			1.04	0.90	0.94	0.94			10		9.80		9.17		9.17	18	0.012	0.0065	561.46	563.42	6.66	PARTIAL FLOW (HGL INSIDE PIPE) DEPTH = 1.09 FT							560.37	562.33	±563	
4+56	4+77	21	B1	1.04	1.04	0.90	0.94	0.94			10		9.80		9.17		9.17	18	0.012	0.0065	563.42	563.59	6.66	PARTIAL FLOW (HGL INSIDE PIPE) DEPTH = 1.09 FT.							562.33	562.50	±564	



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STATE OF TEXAS — JAMES DEWEY, JR. — 61480 — LICENSED PROFESSIONAL ENGINEER

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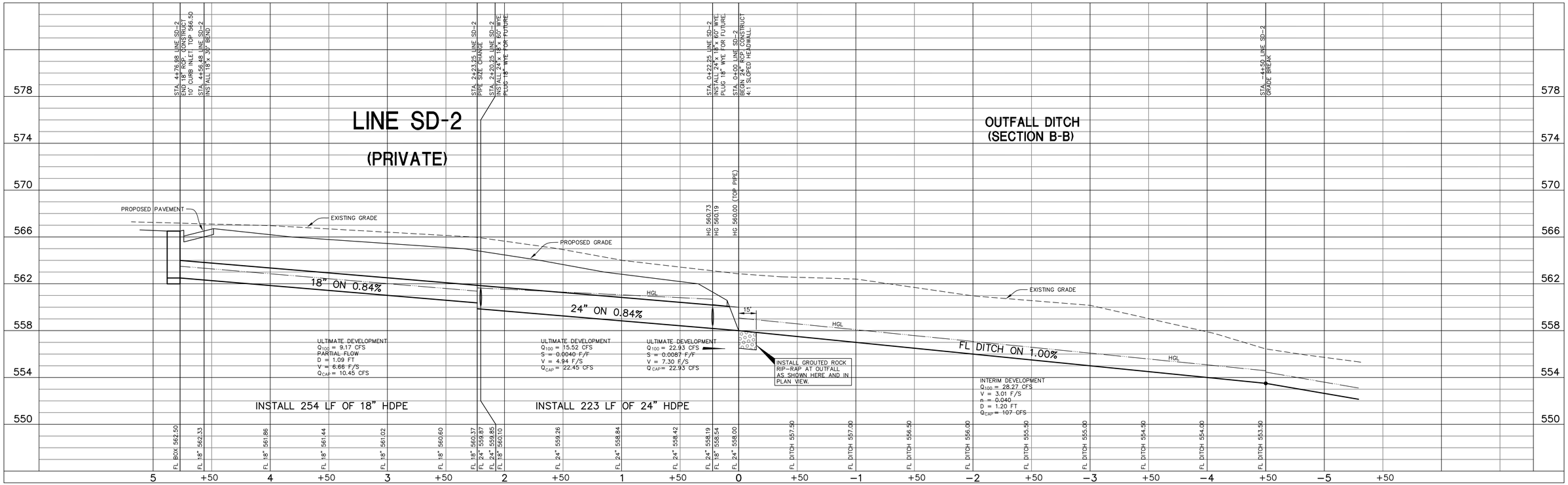

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JDJR FILE NO. 1115-4-14

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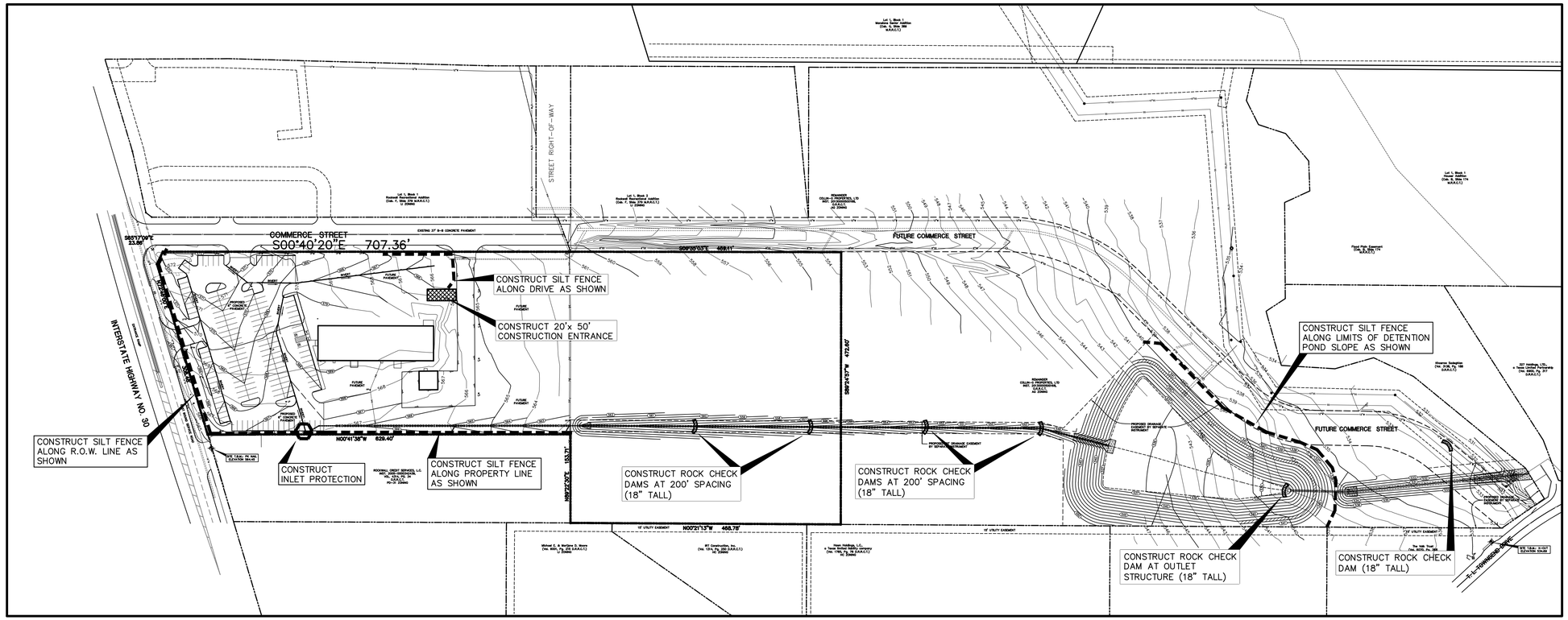


EROSION CONTROL PLAN

NORTH

SCALE: 1" = 100'

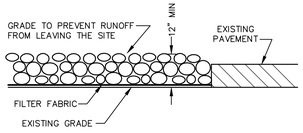
0' 100' 200' 300'

GRAPHIC SCALE

"AS-BUILT"

THIS AS-BUILT DRAWING IS BASED ON INFORMATION OBTAINED FROM BUILDING CONTRACTORS DURING CONSTRUCTION AS PROVIDED TO JDJR ENGINEERS & CONSULTANTS, INC. JDJR ENGINEERS & CONSULTANTS, INC. DOES NOT GUARANTEE THE ACCURACY OR COMPLETENESS OF THESE RECORDS. ALL PROPOSED IMPROVEMENTS WERE CONSTRUCTED EXCEPT FOR THE PARKING LOT ON THE NORTH SIDE OF THE BUILDING. THE EXISTING DRIVE APPROACH FROM THE SERVICE ROAD WAS REMOVED AND THE NEW DRIVE APPROACH CONSTRUCTED.

AREA OF SITE = ±32.7 ACRES
DISTURBED AREA = ±5.0 ACRES

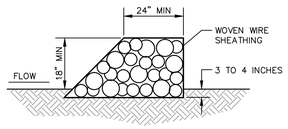


NOTES:

1. STONE SHALL BE 4 TO 6 INCH DIAMETER CRUSHED ROCK. NO CRUSHED PORTLAND CEMENT CONCRETE ALLOWED.
2. LENGTH SHALL BE SHOWN ON PLANS, WITH A MINIMUM LENGTH OF 50 FEET FOR LOTS WHICH ARE LESS THAN 150 FEET FROM THE EDGE OF PAVEMENT. THE MINIMUM DEPTH IN ALL OTHER CASES SHALL BE 50 FEET.
3. THE THICKNESS SHALL NOT BE LESS THAN 12 INCHES.
4. THE WIDTH SHALL BE NO LESS THAN THE FULL WIDTH OF ALL POINTS OF INGRESS OR EGRESS.
5. WHEN NECESSARY, VEHICLES SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO A PUBLIC ROADWAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE WITH DRAINAGE FLOWING AWAY FROM BOTH THE STREET AND THE STABILIZED ENTRANCE. ALL SEDIMENT SHALL BE PREVENTED FROM ENTERING ANY STORM DRAIN, OR WATERCOURSE USING APPROVED METHODS.
6. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PAVED SURFACES. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PAVED SURFACES, MUST BE REMOVED IMMEDIATELY.
7. THE ENTRANCE MUST BE PROPERLY GRADED OR INCORPORATE A DRAINAGE TO PREVENT RUNOFF FROM LEAVING THE CONSTRUCTION SITE.

STABILIZED CONSTRUCTION ENTRANCE

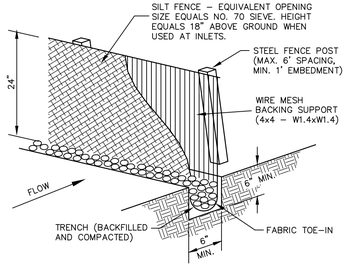
NOT TO SCALE



ROCK BERM NOTES:

1. USE ONLY OPEN GRADED 4–8 INCHES IN DIAMETER FOR STREAM FLOW CONDITIONS. USE OPEN GRADED ROCK 3–5 INCHES IN DIAMETER FOR OTHER CONDITIONS.
2. THE ROCK BERM SHALL BE SECURED WITH A WOVEN WIRE SHEATHING HAVING A MAXIMUM OPENING OF 1 INCH AND A MINIMUM WIRE SIZE OF 20 GUAGE AND SHALL BE BURIED IN A TRENCH APPROXIMATELY 3 TO 4 INCHES DEEP.
3. THE ROCK BERM SHALL BE INSPECTED EVERY TWO WEEKS OR AFTER EACH 1/2" RAIN EVENT AND SHALL BE REPLACED WHEN THE STRUCTURE CEASES TO FUNCTION AS INTENDED DUE TO SILT ACCUMULATION AMONG THE ROCKS, WASHOUT, CONSTRUCTION TRAFFIC DAMAGE, ETC.
4. WHEN SILT REACHES A DEPTH EQUAL TO ONE–THIRD OF THE HEIGHT OF THE BERM OR ONE FOOT, WHICHEVER IS LESS, THE SILT SHALL BE REMOVED AND DISPOSED OF PROPERLY.
5. WHEN THE SITE IS COMPLETELY STABILIZED, THE BERM AND ACCUMULATED SILT SHALL BE REMOVED AND DISPOSED OF IN AN APPROVED MANNER.
6. ROCK BERM SHOULD BE USED AS CHECK DAMS FOR CONCENTRATED FLOW AND ARE NOT INTENDED FOR USE IN PERIMETER PROTECTION.

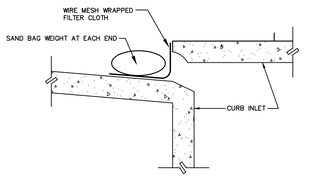
ROCK BERM DETAIL



SILT FENCE DETAIL

SILT FENCE NOTES:

1. STEEL POSTS WHICH SUPPORT THE SILT FENCE SHALL BE INSTALLED ON A SLIGHT ANGLE TOWARD THE ANTICIPATED RUNOFF SOURCE. POST MUST BE EMBEDDED A MINIMUM OF ONE FOOT.
2. THE TOE OF THE SILT FENCE SHALL BE TRENCHED IN WITH A SPADE OR MECHANICAL TRENCHER, SO THAT THE DOWNSLOPE FACE OF THE TRENCH IS FLAT AND PERPENDICULAR TO THE LINE OF FLOW. WHERE FENCE CANNOT BE TRENCHED IN (e.g. PAVEMENT), WEIGHT FABRIC FLAP WITH WASHED GRAVEL ON UPHILL SIDE TO PREVENT FLOW UNDER FENCE.
3. THE TRENCH MUST BE A MIN. OF 6" DEEP AND 6" WIDE TO ALLOW FOR THE SILT FENCE FABRIC TO BE LAID IN THE GROUND AND BACKFILLED WITH COMPACTED MATERIAL.
4. SILT FENCE SHOULD BE SECURELY FASTENED TO EACH STEEL SUPPORT POST OR WOVEN WIRE, WHICH IS IN TURN ATTACHED TO THE STEEL FENCE POST. THERE SHALL BE A 6" OVERLAP, SECURELY FASTENED WHERE ENDS OF FABRIC MEET.
5. INSPECTION SHALL BE MADE WEEKLY OR AFTER EACH RAINFALL. REPAIR OR REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
6. SILT FENCE SHALL BE REMOVED WHEN THE SITE IS COMPLETELY STABILIZED SO AS NOT TO BLOCK OR EMPEDE STORM FLOW OR DRAINAGE.
7. ACCUMULATED SILT SHALL BE REMOVED WHEN IT REACHES A DEPTH OF 6". THE SILT SHALL BE DISPOSED OF IN AN APPROVED SITE AND IN SUCH A MANNER AS TO NOT CONTRIBUTE TO ADDITIONAL SILTATION.



NOTES:
1. WIRE MESH BACKING MUST BE OF SUFFICIENT STRENGTH TO SUPPORT FILTER FABRIC WITH WATER FULLY IMPOUNDED AGAINST IT. FILTER FABRIC TO BE ATTACHED TO WIRE MESH ALONG BOTH EDGES WITH TIE WIRES AT 24" SPACING
2. FILTER CLOTH MUST BE OF A TYPE APPROVED FOR THIS PURPOSE; RESISTANT TO SUNLIGHT WITH SIEVE SIZE, EOS, 40–85, TO ALLOW SUFFICIENT PASSAGE OF WATER AND REMOVAL OF SEDIMENT.
3. FORM THE WIRE MESH AND FILTER CLOTH TO THE CONCRETE GUTTER AND AGAINST THE FACE OF CURB ON BOTH SIDES OF THE INLET.
4. THE ASSEMBLY SHALL BE PLACED, SO THAT THE ENDS OF THE SPACERS ARE A MINIMUM OF 6" BEYOND ENDS OF THE THROAT OPENING. THE FILTER CLOTH SHOULD END 2" BELOW THE TOP OF THE THROAT.
5. THIS TYPE OF INLET PROTECTION MUST BE INSPECTED FREQUENTLY AND THE FILTER CLOTH CLEANED OR REPLACED WHEN CLOGGED WITH SEDIMENT.

CURB INLET PROTECTION DETAIL

GENERAL NOTES:

1. THE GENERAL CONTRACTOR AND OWNER RESPONSIBLE FOR PREVENTING SEDIMENT AND OTHER POLLUTANTS FROM LEAVING THE SITE. CARE SHALL BE EXERCISED TO PREVENT THE FLOW OR OFF–SITE TRACKING OF SEDIMENT AND OTHER POLLUTANTS TO ADJACENT PAVED DRIVEWAYS, ROADWAYS, INLETS AND STORM DRAIN SYSTEM.
2. ALL LOCATIONS USED AS AN EXIT MUST HAVE ROCK STABILIZATION 50' MINIMUM LENGTH, 4–6" DIAMETER STONE OVER GEOTEXTILE FABRIC.
3. THE STABILIZED CONSTRUCTION ENTRY/EXIT SHALL BE USED AS A WHEEL WASH AREA FOR ALL TRUCKS LEAVING THE SITE.
4. A BERM OR OTHER SPILL PROTECTION MEASURE SHALL BE CONSTRUCTED FOR ANY TEMPORARY FUEL STORAGE TANKS ON SITE DURING CONSTRUCTION.
5. ALL TRASH SHALL BE CONTAINED IN AN ENCLOSURE UNTIL PROPER DISPOSAL AT OFF–SITE FACILITIES.
6. VEHICLE PARKING AREAS, STAGING AREAS, STOCKPILES, SPOILS, ETC. SHALL BE LOCATED SUCH THAT THEY WILL NOT ADVERSELY AFFECT STORM WATER QUALITY. OTHERWISE, COVERING OR ENCIRCLING THE AREAS WITH PROTECTIVE MEASURES SHALL BE NECESSARY.
7. A DENSITY OF TEMPORARY OR PERMANENT GROUND COVER (I.E., VEGETATION, EROSION CONTROL, MATTING, ETC.) SUFFICIENT TO PREVENT EROSION SHALL BE ESTABLISHED ON ALL SWALES AND SLOPES IN A TIMELY MANNER IN ORDER TO PREVENT EROSION PROBLEMS FROM DEVELOPING IN THESE AREAS.
8. ALL SURFACE AREAS DISTURBED WITHIN OR ADJACENT TO THE CONSTRUCTION LIMITS MUST BE PERMANENTLY STABILIZED. STABILIZATION IS OBTAINED WHEN THE SITE IS COVERED WITH IMPERVIOUS STRUCTURES, PAVING OR A UNIFORM PERENNIAL VEGETATIVE COVER. THE PERENNIAL VEGETATION MUST HAVE A COVERAGE DENSITY OF AT LEAST 70 PERCENT. STABILIZATION IS REQUIRED BEFORE TERMINATING MAINTENANCE AND REMOVAL OF EROSION CONTROL MEASURES.
9. ALL PERIMETER EROSION CONTROL MEASURES AND A ROCK STABILIZED ENTRY/EXIT MUST BE IN PLACE BEFORE STARTING SOIL DISTURBING ACTIVITIES.
10. EROSION CONTROL MEASURES THAT PROVE TO BE INEFFECTIVE SHALL BE REPLACED WITH MORE EFFECTIVE MEASURES OR ADDITIONAL MEASURES.
11. A MAINTENANCE PROGRAM FOR ALL PROPOSED EROSION CONTROL MEASURES SHALL BE ESTABLISHED.
12. TO PREVENT ENTRY OF SEDIMENT INTO PROPOSED STORM SEWERS DURING CONSTRUCTION, INSTALL PIPE SEDIMENT FILTER OR SEDIMENT FILTER OR SEDIMENT BARRIER AT THE END OF EACH WORK DAY.
13. CONTRACTOR TO CONSTRUCT A PIT OR WASH BASIN ON–SITE FOR WASH–OUT OF CONCRETE TRUCKS.
14. IF PUMPS ARE USED TO REMOVE WATER FROM PONDED AREAS, FILTER THE DISCHARGE TO REMOVE SEDIMENT AND OTHER POLLUTANTS BEFORE THE WATER LEAVES THE SITE OR ENTERS STORM DRAIN SYSTEM. DO NOT BYPASS SILT BARRIERS OR INLET SEDIMENT FILTERS WITH THE DISCHARGE.
15. TO PREVENT DAMAGE TO VEGETATION IN DOWNSTREAM WATER COURSES, LIMIT ANY PROPOSED LIME STABILIZATION OPERATIONS TO THAT WHICH CAN BE MIXED AND COMPACTED BY THE END OF EACH WORK DAY. A SILT FENCE IS NOT EFFECTIVE IN FILTERING LIME SINCE THE GRAIN SIZE IS SIGNIFICANTLY SMALLER THAN THE OPENING IN THE FABRIC.
16. THE CONTRACTOR(S) SHALL INSPECT EROSION CONTROL MEASURES AT LEAST ONCE EACH WEEK AND WITHIN 24 HOURS AFTER A STORM EVENT OF 1/2 INCH OR GREATER. REPAIR OR REPLACE DAMAGED MEASURES AS NECESSARY TO RETAIN SEDIMENT ON SITE. EROSION CONTROL MEASURES THAT PROVE TO BE INEFFECTIVE SHALL BE REPLACED WITH MORE EFFECTIVE MEASURES OR ADDITIONAL MEASURES WITHIN SEVEN (7) CALENDAR DAYS.
17. FOR ALTERNATIVE STABILIZATION AND EROSION CONTROL MEASURES, REFER TO THE CONSTRUCTION BEST MANAGEMENT PRACTICES (BMP) MANUAL PUBLISHED BY NORTH CENTRAL TEXAS COUNCIL OF GOVERNMENTS, 3RD EDITION,.
18. CONTRACTOR TO CHECK AREAS ADJACENT TO PROPERTY DAILY FOR CONSTRUCTION WASTE MATERIALS AND DEBRIS THAT HAVE BLOWN OR WASHED OFF–SITE AND REMOVE IMMEDIATELY.
19. CONTAIN ALL RUNOFF FROM MATERIALS USED IN THE SUBGRADE STABILIZATION PROCESS.

LEGEND

545 — EXISTING CONTOUR LINE

PROPOSED SILT FENCE (STRAW WATTLES MAY BE SUBSTITUTED)

PROPOSED STABILIZED CONSTRUCTION ENTRANCE

PROPOSED INLET CONTROL (WIRE MESH/FILTER FABRIC)

REVISIONS:	
5/19/15	PER CITY REVIEW
8/07/15	PER CITY REVIEW
2/26/16	RELOCATE DRIVEWAY
3/17/16	PER CITY REVIEW
3/23/16	PER CITY REVIEW
4/05/17	AS–BUILT

STATE OF TEXAS — JAMES DEWEY, JR. — 61482 — REGISTERED PROFESSIONAL ENGINEER

04/05/17

SHEET TITLE:

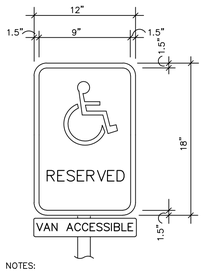
EROSION CONTROL PLAN
YOUNG HYUNDAI
1530 SOUTH INTERSTATE HIGHWAY NO. 30
ROCKWALL, TEXAS

PREPARED BY:
JDJR ENGINEERS & CONSULTANTS, INC.
TSBPE REGISTRATION NUMBER F-8527
ENGINEERS • SURVEYORS • LAND PLANNERS
2500 Texas Drive Suite 100 Irving, Texas 75062
Tel 972-252-5357 Fax 972-252-8958

DATE: MAR. 25, 2015	DRAWN BY: SAS	SHEET NO.
SCALE: 1" = 100'	CHECKED BY: JDJR	C5 OF 6

JDJR FILE NO. 1115–4–14

C:\jdjrproj\2014\1115-4-14 Rockwall Hyundai\Civil\1115-4-14 CIVILS AS BUILTS PHASE 1.dwg, 4/5/2017 5:44:29 PM, DWG To PDF.pc3

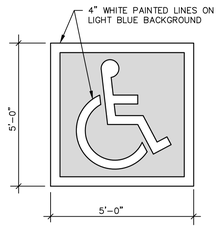


NOTES:

1. ALL LETTERS ARE 1" SERIES 'C'.
2. SIGN BACKGROUND TO BE WHITE WITH BLUE GRAPHICS, LETTERING & BORDER.
3. SIGN TO BE MOUNTED ON GALVANIZED STEEL POST SET IN CONCRETE. BOTTOM OF SIGN AT 60" ABOVE PAVING–REF. LOCAL CODES AND TEXAS ACCESSIBILITY STANDARDS.
4. VAN ACCESSIBLE SIGN TO BE ATTACHED TO SIGN POST AT DESIGNATED SPACE.

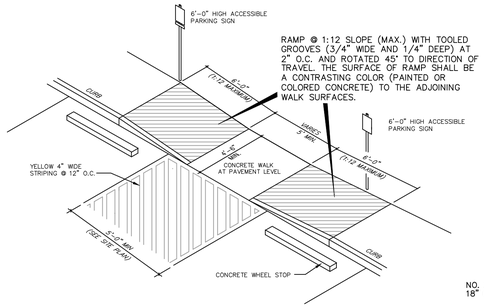
ACCESSIBLE PARKING SIGN DETAIL

NO SCALE



ACCESSIBLE PARKING PAINTED SYMBOL DETAIL

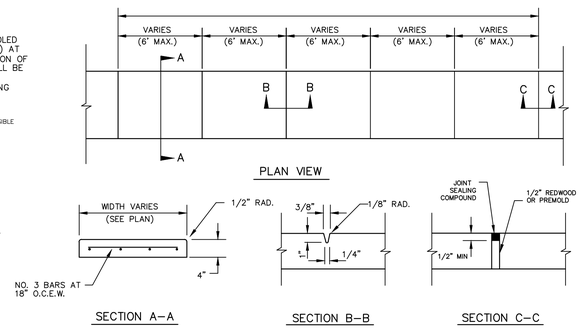
NO SCALE



NOTE: ACCESSIBLE PARKING AREAS TO BE IN ACCORDANCE TO THE TEXAS ACCESSIBILITY STANDARDS (TAS) FOR SIGNS, RAMPS, MARKINGS, ACCESSIBLE ROUTES ETC.

PRIVATE ACCESSIBLE RAMP AT PARKING DETAIL

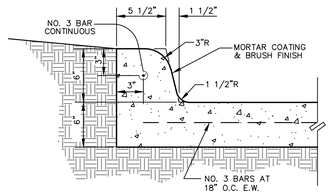
NO SCALE



NOTE: ALL WALKS TO BE MINIMUM 3,500 PSI CONCRETE PLACED ON COMPACTED BASE.

ON–SITE CONCRETE WALK DETAILS

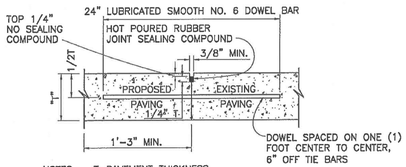
NOT TO SCALE:



NOTE: TRANSVERSE SLOPE AT THE CURB VARIES – SEE PLAN VIEW

INTEGRAL CURB DETAIL

NOT TO SCALE



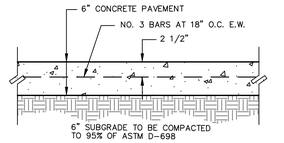
NOTES: T=PAVEMENT THICKNESS

1. LONGITUDINAL BUTT CONSTRUCTION MAY BE UTILIZED IN PLACE OF LONGITUDINAL HINGED (KEYWAY) JOINT AT CONTRACTORS OPTION.
2. DOWEL BARS SHALL BE DRILLED INTO PAVEMENT HORIZONTALY BY USE OF A MECHANICAL RIG.

   DRILLING BY HAND IS NOT ACCEPTABLE, PUSHING DOWEL BARS INTO GREEN CONCRETE NOT ACCEPTABLE

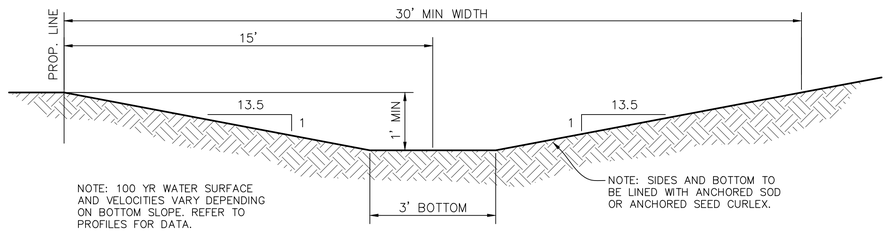
LONGITUDINAL BUTT JOINT

NOT TO SCALE



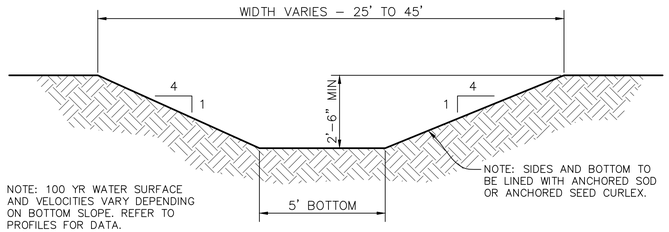
CONCRETE PAVEMENT SECTION DETAIL

NOT TO SCALE



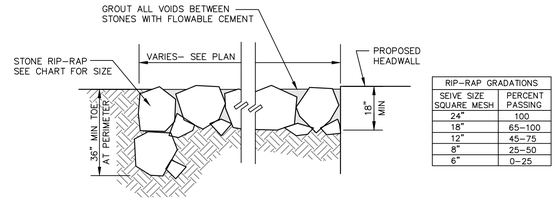
SECTION A–A (3' BOTTOM DITCH)

NOT TO SCALE



SECTION D–D (5' BOTTOM DITCH)

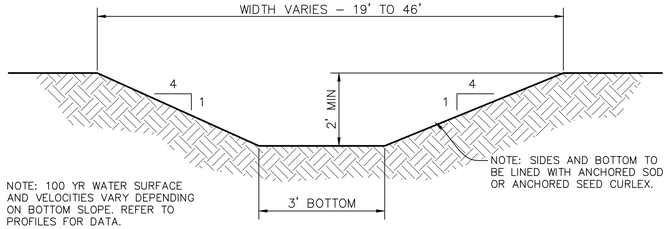
NOT TO SCALE



RIP–RAP GRADATIONS	
SEIVE SIZE SQUARE MESH	PERCENT PASSING
24"	100
18"	65–100
12"	45–75
8"	25–50
6"	0–25

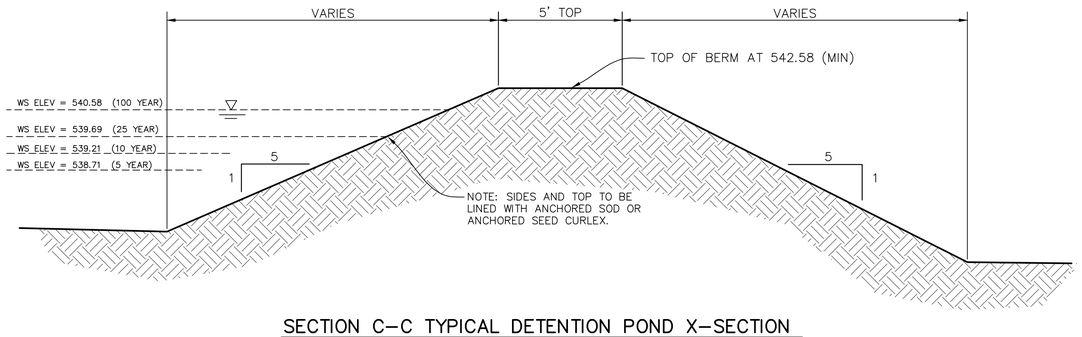
GROUTED ROCK RIP–RAP DETAIL

NOT TO SCALE



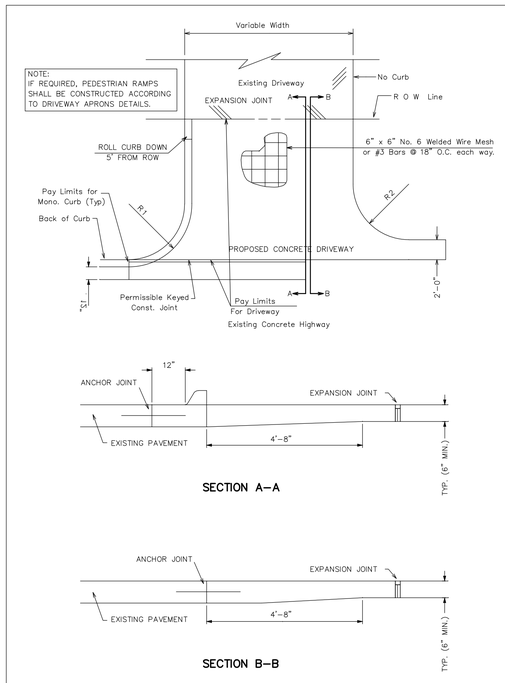
SECTION B–B (3' BOTTOM DITCH)

NOT TO SCALE



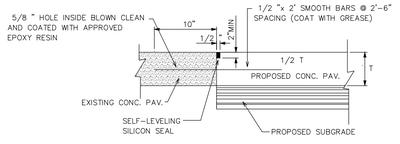
SECTION C–C TYPICAL DETENTION POND X–SECTION

NOT TO SCALE



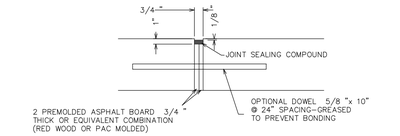
GENERAL NOTES:

1. DRIVEWAYS THAT REQUIRE PEDESTRIAN RAMPS SHALL BE CONSTRUCTED ACCORDING TO TXDOT STANDARD (RAMP 00–B).
2. CONCRETE SHALL BE CLASS A BEING SACK, WITH MINIMUM COMPRESSIVE STRENGTH OF 3000 PSI.
3. MINIMUM DRIVEWAY THICKNESS IS 6".
4. ALL TRAFFIC CONTROL MUST CONFORM TO THE TEXAS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. PART IV IN PARTICULAR.
5. ALL CONSTRUCTION MUST CONFORM TO TEXAS DEPARTMENT OF TRANSPORTATION SPECIFICATIONS AND TO CITY SPECIFICATIONS IN WHICH IN WHICH THE WORK IS TO BE PERFORMED.
6. MEASURES SHALL BE TAKEN TO PREVENT EROSION DURING ALL STAGE OF CONSTRUCTION.
7. ANY DISTURBED VEGETATION SHALL BE REPLACED TO IT ORIGINAL CONDITION.
8. IF EXISTING PAVEMENT IS 8" OR LESS A # 4 BAR WILL BE USED, GREATER THAN 8" # 5 BAR.



ANCHOR JOINT DETAIL

TO BE USED WHERE PROPOSED CONCRETE PAVEMENT MEET EXISTING CONCRETE PAVEMENT



EXPANSION JOINT DETAIL

Texas Department of Transportation

DALLAS NORTHWEST AREA OFFICE

DRIVEWAY DETAILS

"AS-BUILT"

THIS AS-BUILT DRAWING IS BASED ON INFORMATION OBTAINED FROM BUILDING CONTRACTORS DURING CONSTRUCTION AS PROVIDED TO JDJR ENGINEERS & CONSULTANTS, INC.. JDJR ENGINEERS & CONSULTANTS, INC. DOES NOT GUARANTEE THE ACCURACY OR COMPLETENESS OF THESE RECORDS. ALL PROPOSED IMPROVEMENTS WERE CONSTRUCTED EXCEPT FOR THE PARKING LOT ON THE NORTH SIDE OF THE BUILDING. THE EXISTING DRIVE APPROACH FROM THE SERVICE ROAD WAS REMOVED AND THE NEW DRIVE APPROACH CONSTRUCTED.

REVISIONS:	
5/19/15	PER CITY REVIEW
8/07/15	PER CITY REVIEW
2/26/16	RELOCATE DRIVEWAY
3/17/16	PER CITY REVIEW
4/05/17	AS–BUILT

STATE OF TEXAS
JAMES DEWEY, JR.
61482
REGISTERED PROFESSIONAL ENGINEER
05/18/2016

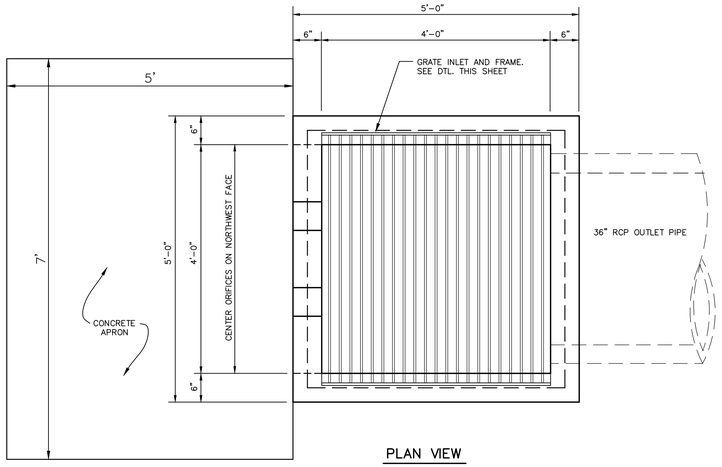
SHEET TITLE:

SITE AND PAVING DETAILS
YOUNG HYUNDAI
1530 SOUTH INTERSTATE HIGHWAY NO. 30
ROCKWALL, TEXAS

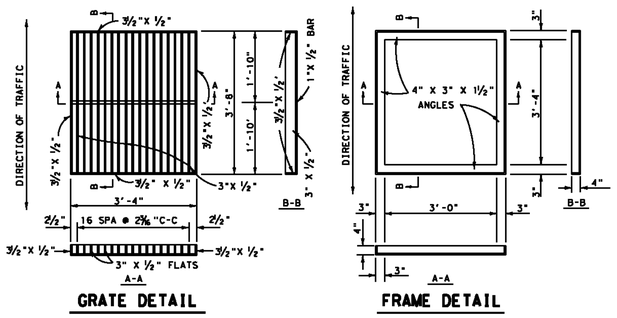
JDJR
PREPARED BY:
ENGINEERS & CONSULTANTS, INC.
TSBPE REGISTRATION NUMBER F-8527
ENGINEERS • SURVEYORS • LAND PLANNERS
2500 Texas Drive Suite 100 Irving, Texas 75062
Tel 972-252-5357 Fax 972-252-8958

DATE:	MAR. 25, 2015	DRAWN BY:	SAS	SHEET NO.
SCALE:	AS SHOWN	CHECKED BY:	JDJR	C6A OF 6

JDJR FILE NO. 1115–4–14

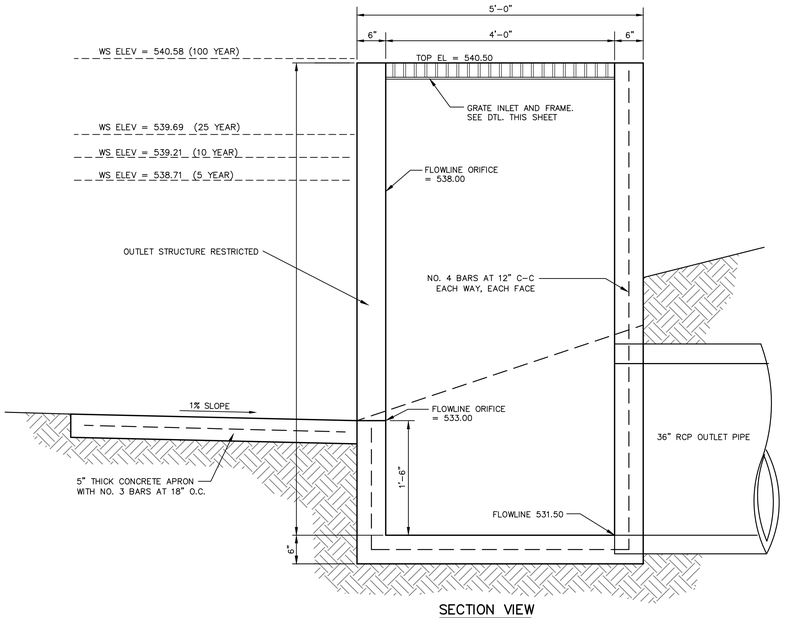


PLAN VIEW

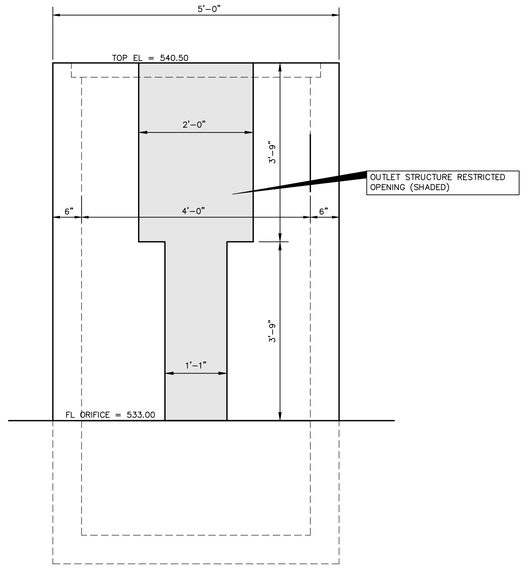


FRAME AND GRATE DETAILS

NOT TO SCALE



SECTION VIEW

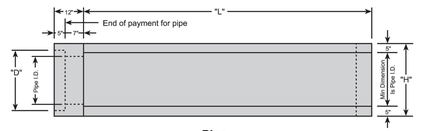


OUTFALL ORIFICE DETAIL
(NORTHWEST SIDE OF BOX)

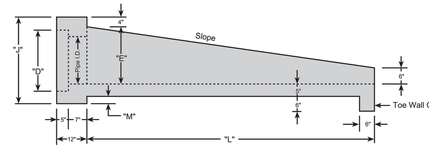
CAST IN PLACE Y-INLET DETAILS
DETENTION POND OUTFALL STRUCTURE

NOT TO SCALE

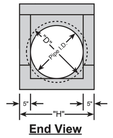
Precast Drainage Structures



Plan



Longitudinal Elevation



End View

Basic Dimensions

Pipe Dia. (In.)	Slope	"D" (In.)	"E" (In.)	"H" (In.)	"J" (In.)	"L" (In.)	"M" (In.)	Weight (Lbs.)
12	3:1	17.000	17.500	28.00	26.250	34.500	0.75	1,328
	4:1	17.000	17.500	28.00	26.250	46.000	0.75	1,558
	6:1	17.000	17.500	28.00	26.250	69.000	0.75	2,017
15	3:1	20.500	17.500	28.00	26.250	34.500	0.75	1,243
	4:1	20.500	17.500	28.00	26.250	46.000	0.75	1,472
	6:1	20.500	17.500	28.00	26.250	69.000	0.75	1,931
18	3:1	24.000	21.000	30.00	30.000	45.000	1.00	1,751
	4:1	24.000	21.000	30.00	30.000	60.000	1.00	2,018
	6:1	24.000	21.000	30.00	30.000	90.000	1.00	2,831
24	3:1	31.000	27.500	37.00	37.000	64.500	1.50	2,831
	4:1	31.000	27.500	37.00	37.000	86.000	1.50	3,596
	6:1	31.000	27.500	37.00	37.000	129.000	1.50	4,818
30	3:1	38.500	34.500	41.00	44.500	85.500	2.25	4,092
	4:1	38.500	34.500	41.00	44.500	114.000	2.25	5,082
	6:1	38.500	34.500	41.00	44.500	171.000	2.25	7,060
36	3:1	45.500	41.000	47.00	51.500	105.000	2.50	5,544
	4:1	45.500	41.000	47.00	51.500	140.000	2.50	6,827
	6:1	45.500	41.000	47.00	51.500	210.000	2.50	8,722

Note:

1.) The Safety End Treatment option shown on this sheet should only be utilized in the following instances:
   A.) On cross-drainage structures for 36" or smaller diameter pipes.
   B.) On parallel-drainage structures for 30" or smaller pipes.
2.) Cross & Parallel drainage structures larger than the requirements listed above shall utilize other safety end treatment designs shown elsewhere in the plans.
3.) All exposed corners shall be chamfered 3/4".
4.) Manufacture of this product shall conform to the requirements of Item 467 except that minimum reinforcing shall be #4 @ 6" (Grade 40) or #4 @ 9" (Grade 60) each way or 6 x 6 - W12 x W12 or 5 x 5 - W10 x W10 welded wire fabric.
5.) Concrete for precast (steel formed) sections shall be a minimum of Class "C" ~~f'c=3,000 psi~~ 4,200 PSI F POURED IN PLACE. (MIN. 7.5 SACK MIX)
6.) At the option and expense of the contractor, the next larger size of Safety End Treatment may be furnished as long as the "D" dimension cast is that of the required size of pipe.

-No Scale-
All dimensions subject to allowable specification tolerances.

TITLE	PLANT	STATE	SECTION-PAGE	DATE
Precast Safety End Treatment For Reinforced Concrete Pipe	Grand Prairie Houston	TX	7.4	08-15-06

Hanson

"AS-BUILT"

THIS AS-BUILT DRAWING IS BASED ON INFORMATION OBTAINED FROM BUILDING CONTRACTORS DURING CONSTRUCTION AS PROVIDED TO JDJR ENGINEERS & CONSULTANTS, INC.. JDJR ENGINEERS & CONSULTANTS, INC. DOES NOT GUARANTEE THE ACCURACY OR COMPLETENESS OF THESE RECORDS. ALL PROPOSED IMPROVEMENTS WERE CONSTRUCTED EXCEPT FOR THE PARKING LOT ON THE NORTH SIDE OF THE BUILDING. THE EXISTING DRIVE APPROACH FROM THE SERVICE ROAD WAS REMOVED AND THE NEW DRIVE APPROACH CONSTRUCTED.

REVISIONS:	
5/19/15	PER CITY REVIEW
8/07/15	PER CITY REVIEW
2/26/16	RELOCATE DRIVEWAY
4/05/17	AS-BUILT

STATE OF TEXAS
JAMES DEWEY, JR.
61458
LICENSED PROFESSIONAL ENGINEER
04/05/17

SHEET TITLE:

STORM DRAIN DETAILS
YOUNG HYUNDAI
1530 SOUTH INTERSTATE HIGHWAY NO. 30
ROCKWALL, TEXAS

PREPARED BY:
JDJR ENGINEERS & CONSULTANTS, INC.
TSBPE REGISTRATION NUMBER F-8527
ENGINEERS • SURVEYORS • LAND PLANNERS
2500 Texas Drive Suite 100 Irving, Texas 75062
Tel 972-252-5357 Fax 972-252-8958

DATE: MAR. 25, 2015	DRAWN BY: SAS	SHEET NO.
SCALE: AS SHOWN	CHECKED BY: JDJR	C6B OF 6

JDJR FILE NO. 1115-4-14