

# HEAVENLY HANDS BIRTHING CENTER

## 908 N. GOLIAD - ROCKWALL, TEXAS

### PARKING LOT IMPROVEMENTS

#### SHEET INDEX

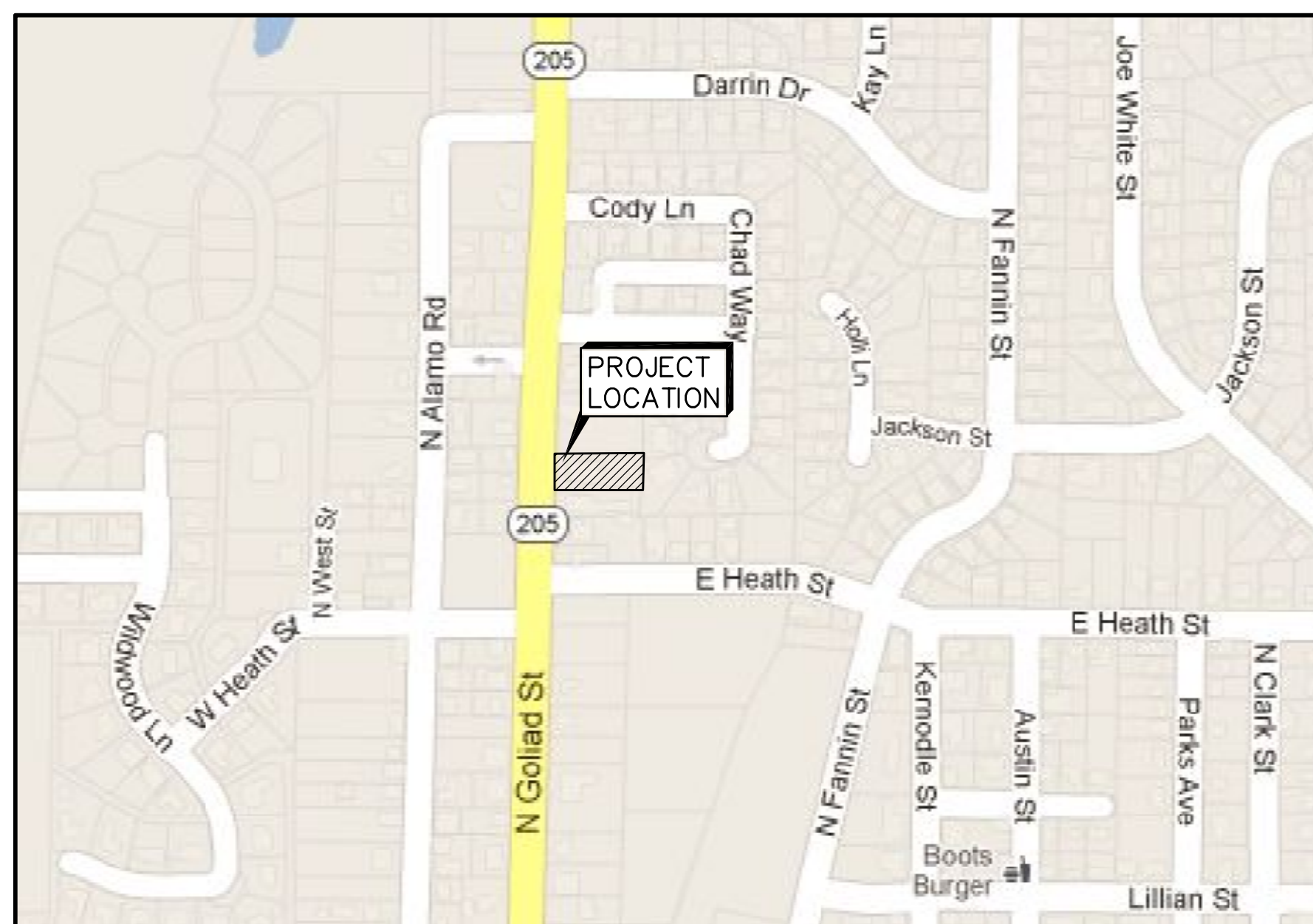
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- 2 SITE PLAN
- 3 DRAINAGE AREA MAP
- 4 GRADING PLAN
- 5 PAVING PLAN
- 6 EROSION CONTROL PLAN

#### TxDOT DETAIL

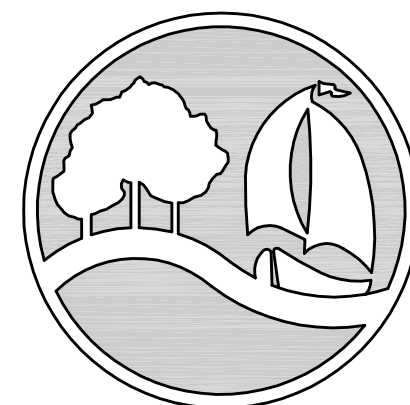
- CCCG-12 CONCRETE CURB AND CURB & GUTTER
- TCP (1-1)-12 TRAFFIC CONTROL PLAN  
CONVENTIONAL ROAD SHOULDER WORK
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POLLUTION CONTROL MEASURES

#### NOTE:

CITY OF ROCKWALL ENGINEERING DEPARTMENT LATEST STANDARD DETAILS AND NCTCOG 3RD EDITION SHALL BE USED FOR CONSTRUCTION OF THIS PROJECT UNLESS OTHERWISE NOTED WITHIN THESE PLANS. THE CONTRACTOR SHALL OBTAIN THE STANDARD DETAIL BOOK FROM THE CITY'S ENGINEERING DEPARTMENT.



VICINITY MAP  
N.T.S.



City of Rockwall  
The New Horizon

PREPARED BY :

**LAM CONSULTING ENGINEERING**  
6804 WILHELMINA DRIVE  
SACHSE, TEXAS 75048  
(214) 766-1011  
www.lamcivil.com  
Firm# F-9763

#### AS-BUILT PLANS

TO THE BEST OF OUR KNOWLEDGE LAM CONSULTING ENGINEERING, HEREBY STATES THAT THIS PLAN IS AS-BUILT. THIS INFORMATION PROVIDED IS BASED ON THE OWNER SURVEYOR SURVEYING AT THE SITE.  
04/29/13



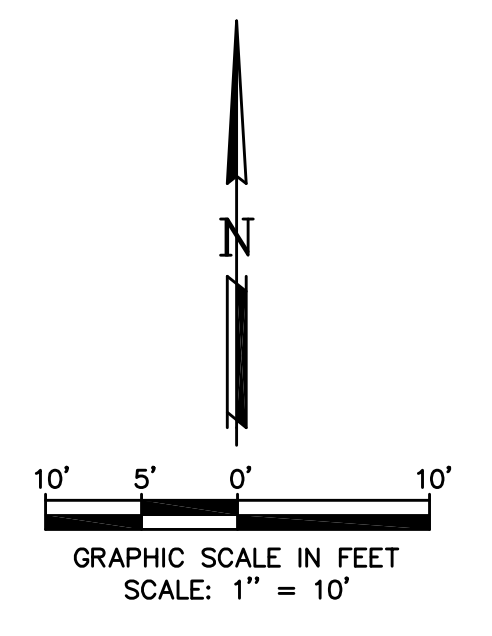
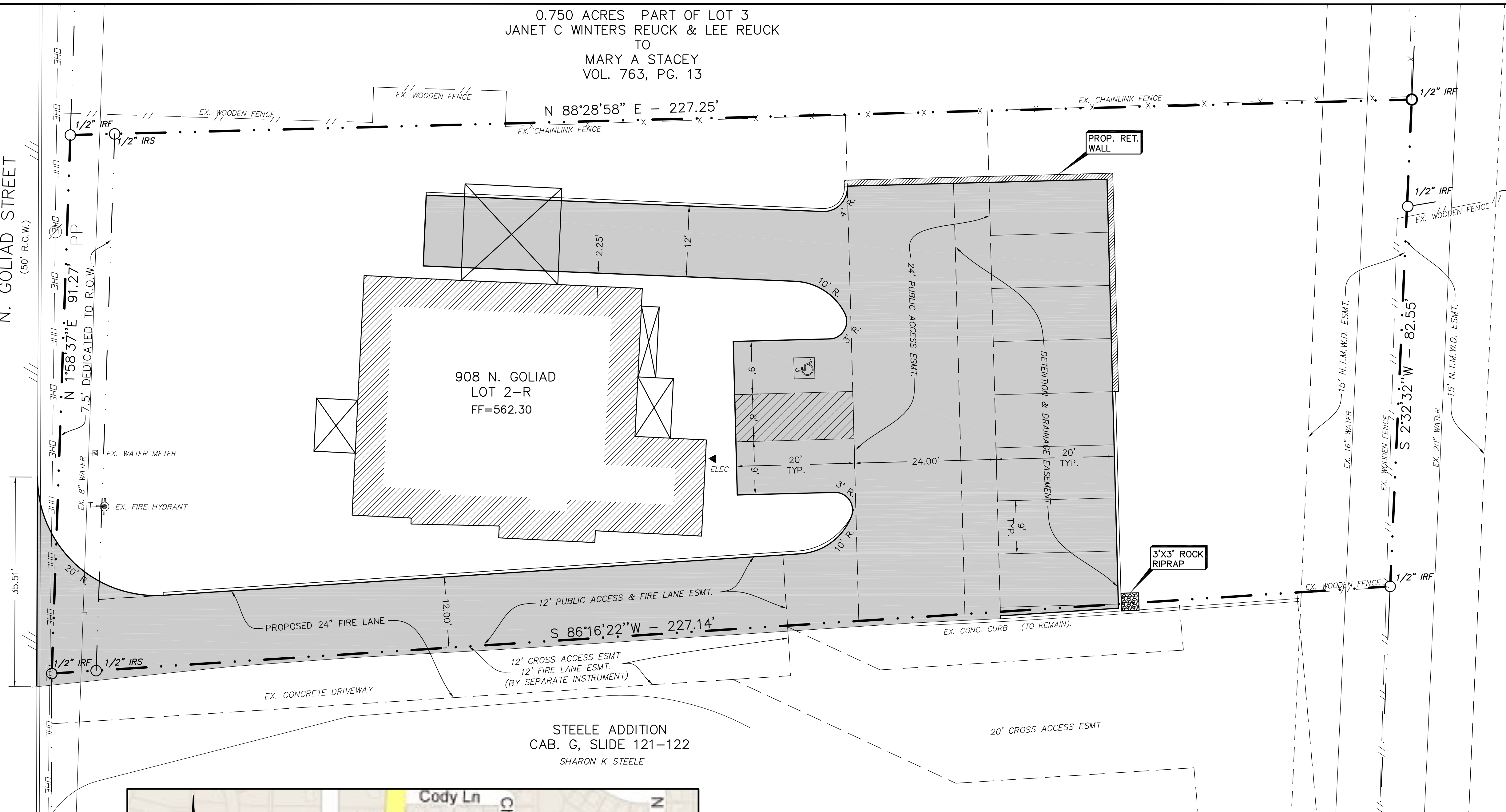
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SURVEY INFORMATION ON THIS DRAWING WAS PROVIDED BY OTHERS. LAM CONSULTING ENGINEERING IS NOT RESPONSIBLE FOR THE ACCURACY OF THE TOPOGRAPHIC MAPPING NOR BENCHMARK DATUM.

THE INFORMATION SHOWN ON THIS SITE PLAN ARE FROM RECORD DRAWINGS. THE ENGINEER IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION SHOWN ON THIS DRAWING. CONTRACTOR SHALL FIELD VERIFY ALL UTILITY AND CONTACT ALL UTILITY COMPANIES 48 HRS PRIOR TO ANY EXCAVATION. 1-800-DIG TESS

0.750 ACRES PART OF LOT 3  
 JANET C WINTERS REUCK & LEE REUCK  
 TO  
 MARY A STACEY  
 VOL. 763, PG. 13

STATE HIGHWAY 205  
 N. GOLIAD STREET  
 (50' R.O.W.)



**SITE DATA**  
 ADDITION = W.D. AUSTIN ADDITION LOT 2-R  
 ZONED = PD50-R0  
 PROPOSED USE = MEDICAL/RESIDENTIAL  
 LOT AREA = 19,671 SF. ~ 0.451 AC.  
 BUILDING AREA = 2,175 SF.  
 PORCH AREA = 428 SF.  
 PAVING AREA = 6,648 SF.  
 LANDSCAPE AREA = 10,420 SF.

**PARKING**  
 PARKING REQUIRED = 10 SPACES  
 PARKING PROVIDED = 10 SPACES

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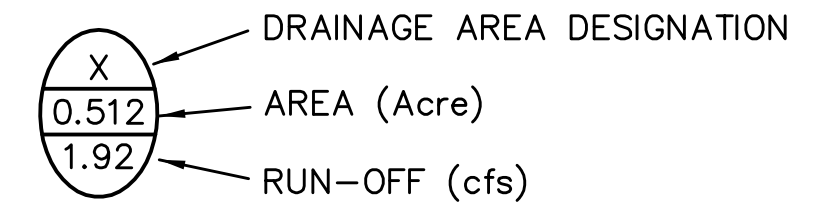
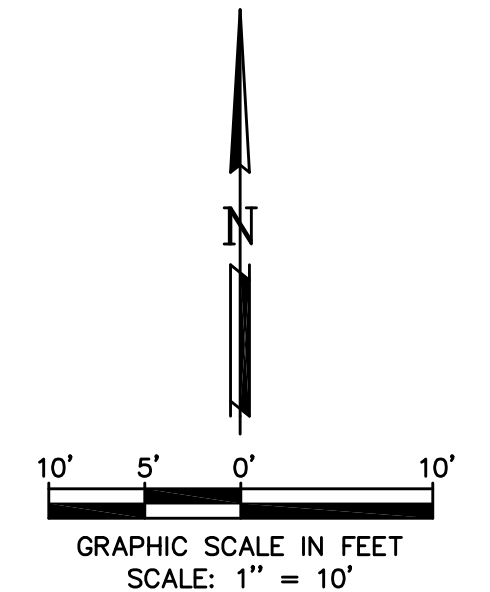
VICINITY MAP  
 N.T.S.

CONTRACTOR SHALL PAY ALL PERMIT FEES AND WILL SETUP PRE-CONSTRUCTION MEETING PRIOR TO COMMENCING ANY WORK.

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<b>SITE PLAN</b>			
<b>W.D. AUSTIN ADDITION - LOT 2-R HEAVENLY HANDS BIRTHING CENTER 908 N. GOLIAD STREET ROCKWALL, TEXAS 75087</b>			
<b>LAM CONSULTING ENGINEERING</b> 6804 WILHELMINA DRIVE SACHSE, TEXAS 75048 Phone (214) 766-1011 www.lamcivil.com Firm #F-9763			
DESIGN: LCE	CHECK: LCE	SCALE: AS NOTED	2 of 6
DRAWN: CTL	DATE: JANUARY 2013	PROJECT: 0428-13	

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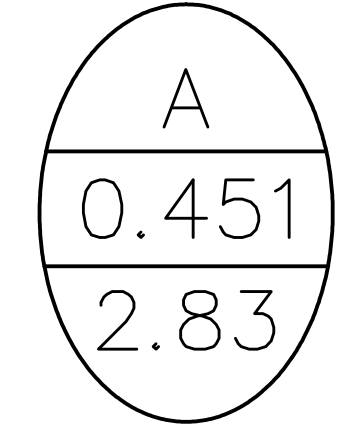
**DRAINAGE AREA MAP**

W.D. AUSTIN ADDITION - LOT 2-R  
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 908 N. GOLIAD STREET  
 ROCKWALL, TEXAS 75087

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STATE HIGHWAY 205  
 N. GOLIAD STREET  
 (50' R.O.W.)



908 N. GOLIAD  
 LOT 2-R  
 FF=562.30

DETENTION AREA  
 (932 C.F.)

3" WIDE CURB  
 OPENING W/ 3'X3'  
 GROUTED ROCK  
 RIPRAP (4"-6" DIA.)

TEMP. BENCHMARK  
 "X" CUT IN CONCRETE CURB  
 ELEV. = 556.25

STEELE ADDITION  
 CAB. G, SLIDE 121-122  
 SHARON K STEELE

**DETENTION CALCULATION:**  
 Based on 100-yr storm requirements

Present Conditions (Residential)	Time	I	Q(cfs)	Vol (cf) Req.
Q (cfs)	0.75	15	9.00	1.24
C	0.50	20	8.30	1.14
I	9.80	30	6.90	0.95
A	0.153	40	5.80	0.80
Tc	10	50	5.00	0.69
		60	4.50	0.62
Future Conditions (Med./Residential)	70	4.00	0.55	514
Q (cfs)	1.35	80	3.70	0.51
C	0.90	90	3.50	0.48
I	9.80			
A	0.153			
Tc	10			

DETENTION CALCULATIONS (ONLY PROPOSED PAVING AND BUILDING)  
 PAVING AREA = 6,648 SF.  
 TOTAL IMPREVIOUS AREA = 6,648 SF. ~ 0.153 Ac.  
**DETENTION PROVIDED**  
 PARKING LOT:  
 DETENTION AREA = 26'x71.68' = 1863.68 S.F.  
 AVG. DEPTH = (0.0'+1.0')/2 = 0.50'  
 DETENTION STORAGE VOL. = 1863.68x0.50 = 932 C.F.  
 THUS 932 C.F. > 811 C.F.

**DRAINAGE CALCULATION**

DRAINAGE AREA	ACRES	TIME (MIN.)	* RUN-OFF COEF. (C)	INTENSITY (100-YR)	Q100 (cfs)
A	0.451	10	0.64	9.80	2.83 cfs

\*C = (9251/19671)x0.80+(10420/19671)x0.50 = 0.64

**Pre-Development**

Storm	Time	C	I	A	Q (cfs)
10-Yr	10	0.5	7.35	0.153	0.56
25-Yr	10	0.5	8.35	0.153	0.64
50-Yr	10	0.5	9.00	0.153	0.69
100-Yr	10	0.5	9.80	0.153	0.75

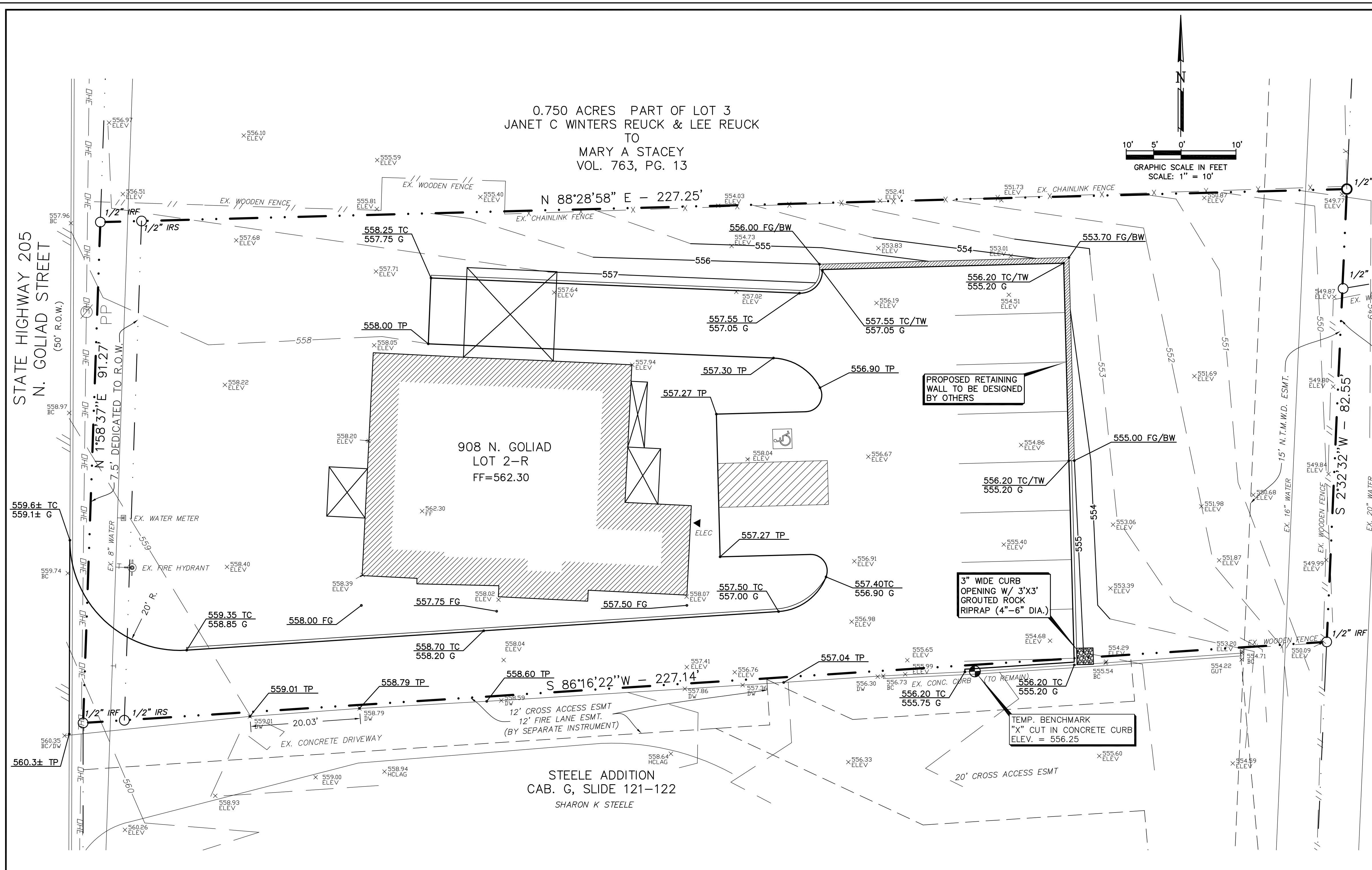
**Post-Development**

Storm	Time	C	I	A	Q (cfs)	Actual Q (cfs)	Actual Velocity (Ft/Sec)
10-Yr	10	0.64	7.35	0.153	0.72	0.56	2.81
25-Yr	10	0.64	8.35	0.153	0.82	0.64	2.90
50-Yr	10	0.64	9.00	0.153	0.88	0.69	2.99
100-Yr	10	0.64	9.80	0.153	0.96	0.75	3.00

Discharge Control Device - Rectangular Contracted weir

Rectangular Contracted weir

Length (Ft)	Height (Ft)	Flow (cfs)	Year
0.25	0.80	0.56	10
0.25	0.88	0.64	25
0.25	0.92	0.69	50
0.25	1.00	0.75	100

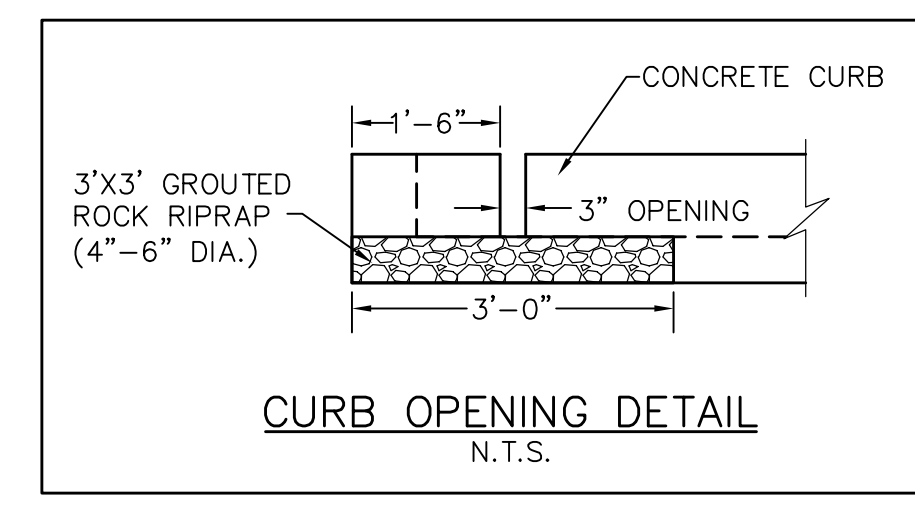
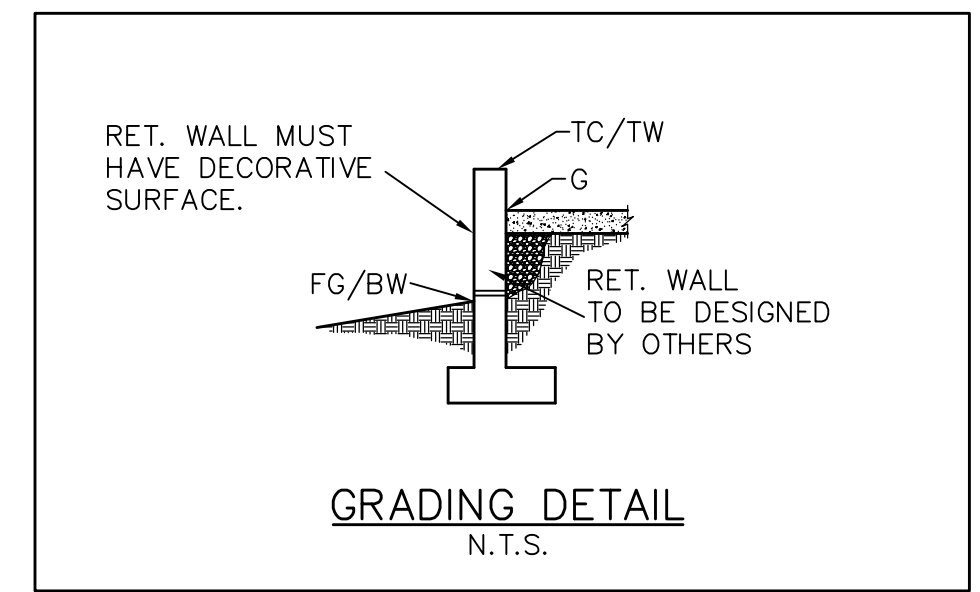


- GRADING NOTES:**
1. ALL MATERIALS AND CONSTRUCTION SHALL CONFORM TO THE CITY'S PLANS AND SPECIFICATIONS, EXCEPT AS NOTED HEREIN AND APPROVED BY THE CITY AND NCTCOG 3RD EDITION.
  2. THE LOCATION OF ALL UTILITIES LOCATED ON THESE PLANS ARE TAKEN FROM EXISTING PUBLIC RECORDS. THE EXACT LOCATION AND ELEVATION OF ALL PUBLIC UTILITIES MUST BE DETERMINED BY THE CONTRACTOR. IT SHALL BE THE DUTY OF THE CONTRACTOR TO ASCERTAIN WHETHER ANY ADDITIONAL FACILITIES OTHER THAN THOSE SHOWN ON THE PLANS MAY BE PRESENT.
  3. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL PUBLIC UTILITIES IN THE CONSTRUCTION OF THIS PROJECT. ALL MANHOLES, CLEAN-OUTS, VALVE BOXES, FIRE HYDRANTS, ETC. MUST BE ADJUSTED TO PROPER LINE AND GRADE BY THE CONTRACTOR PRIOR TO AND AFTER THE PLACING OF PERMANENT PAVING. UTILITIES MUST BE MAINTAINED TO PROPER LINE AND GRADE DURING CONSTRUCTION OF THE PAVING FOR THIS DEVELOPMENT.
  4. DRAINAGE SHOULD BE MAINTAINED AWAY FROM THE FOUNDATIONS, BOTH DURING AND AFTER CONSTRUCTION.
  6. GEOTECH REPORT TO BE PROVIDED BY OTHERS. ALL RECOMMENDATIONS CONTAINED WITHIN GEOTECHNICAL REPORT SHALL BE FOLLOWED IN THE CONSTRUCTION OF THIS PROJECT. ALL EARTHWORK OPERATIONS SHALL CONFORM TO THE RECOMMENDATIONS PER THE GEOTECHNICAL REPORT.
  7. THE SUBGRADE IN AREAS TO BE FILLED SHOULD BE STRIPPED OF VEGETATION AND ANY MAJOR ROOT SYSTEMS. IT SHOULD THEN BE PROOFROLLED WITH HEAVY PNEUMATIC EQUIPMENT. ANY SOFT PUMPING AREAS SHOULD BE UNDERCUT AND PROPERLY BACKFILLED. THE SUBGRADE SHOULD THEN BE SCARIFIED TO A MINIMUM +2 PERCENTAGE POINTS ABOVE THE SOIL'S OPTIMUM MOISTURE DETERMINED BY THAT TEST.
  8. ALL BACKFILL SHALL BE COMPACTED TO 95% PROCTOR DENSITY. BACKFILL SHOULD BE STRIPPED OF VEGETATION AND ANY MAJOR ROOT SYSTEMS. IT SHOULD THEN BE PROOFROLLED WITH HEAVY PNEUMATIC EQUIPMENT. ANY SOFT PUMPING AREAS SHOULD BE UNDERCUT AND PROPERLY BACKFILLED. THE BACKFILL SHOULD THEN BE SCARIFIED TO A MINIMUM +2 PERCENTAGE POINTS ABOVE THE SOIL'S OPTIMUM MOISTURE DETERMINED BY THAT TEST. THE FILL MATERIALS SHOULD THEN BE SPREAD IN LOOSE LIFTS, LESS THAN 8 INCHES THICK, AND UNIFORMLY COMPACTED TO THE SAME CRITERIA. IF FILLING IS SUSPENDED AND THE SUBGRADE BECOMES DESICCATED OR RUTTED, IT SHOULD BE REWORKED PRIOR TO PLACEMENT OF A SUBSEQUENT LIFT. ALL FILL TO BE COMPACTED USING A SHEEP FOOT ROLLER.
  9. COMPACTION TEST SHALL BE DONE FOR EACH 8" OF FILL, BUT NOT LESS THAN ONE TEST FOR EVERY CUBIC YARDS, OR MORE FREQUENTLY IF REQUIRED BY THE SOILS ENGINEER.
  10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY ADDITIONAL COSTS INCURRED FOR THE INSPECTION AND TESTING OF SOILS DUE TO FAILURE TO COMPLY WITH THE MINIMUM REQUIREMENTS OF THE SOILS REPORT.
  11. ALL PROPOSED SPOT SHOTS ARE TOP OF PAVEMENT ELEVATIONS UNLESS OTHERWISE SPECIFIED.
  12. ALL DISTURBED AREAS SHALL BE STABILIZED WITH BROADCAST SEED AND FERTILIZER UPON COMPLETION OF GRADING AND PAVING. CONTRACTOR SHALL PROVIDE WATER AS NECESSARY TO ESTABLISH PERMANENT VEGETATION. 75%-80% OF ALL DISTURBED AREA TO HAVE 1" TALL ESTABLISHED GRASS PRIOR TO ENGINEERING ACCEPTANCE. ALL RIGHT-OF-WAYS TO BE SODDED.

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- LEGEND**
- 450.00 TP TOP OF PAVING ELEVATION
  - 450.00 FG/BW FINISHED GRADE/ BOTTOM OF WALL
  - 450.00 TC/TW TOP OF CURB/ TOP OF WALL ELEVATION
  - 450.00 TC TOP OF CURB ELEVATION
  - 450.00 G GUTTER ELEVATION
  - 485- EXISTING CONTOUR
  - 485- PROPOSED CONTOUR
  - RIDGE — PROPOSED GRADE HIGH POINT
  - PROPOSED FLOW ARROW
  - - - - - PROPOSED VALLEY GUTTER

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**GRADING PLAN**

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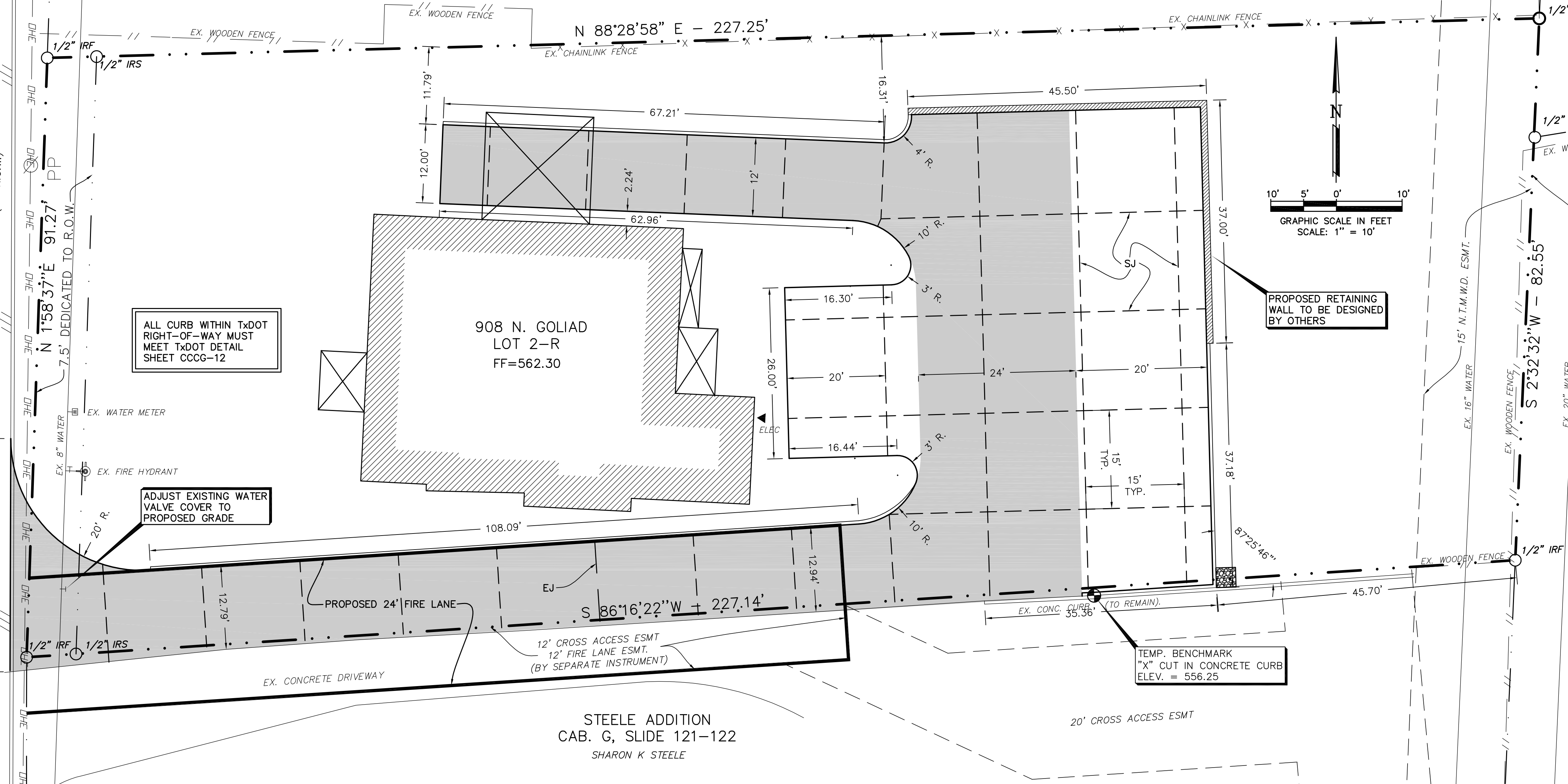
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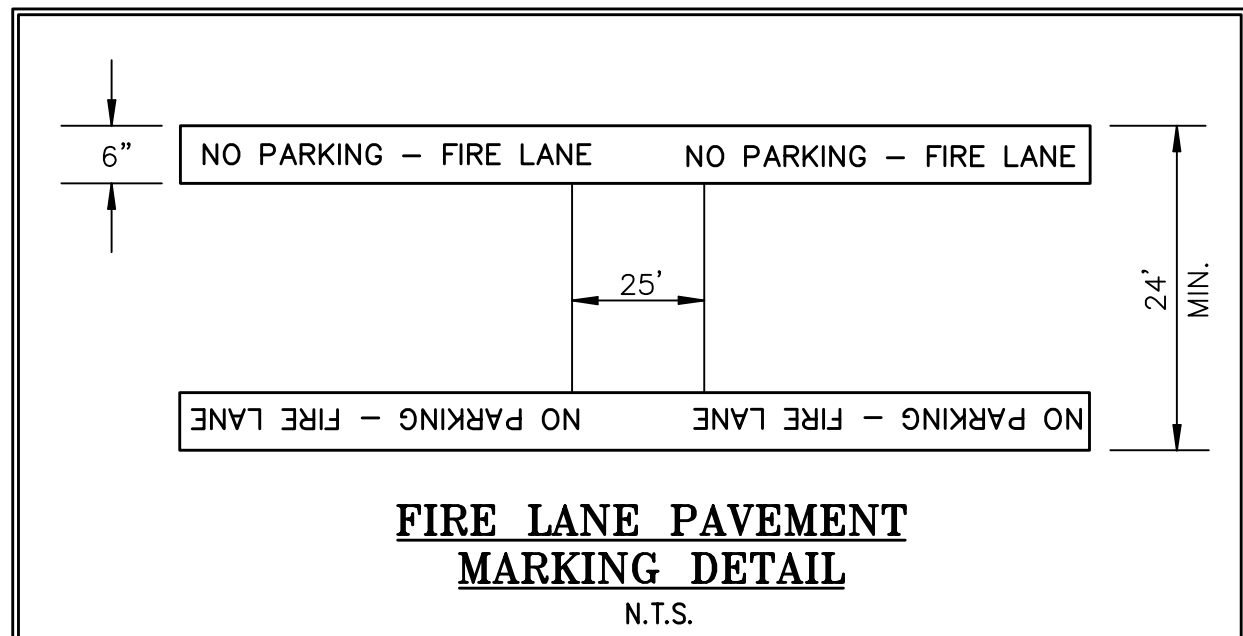
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 TO  
 MARY A STACEY  
 VOL. 763, PG. 13

STATE HIGHWAY 205  
 N. GOLIAD STREET  
 (50' R.O.W.)



- PAVING NOTES**
- ALL MATERIALS AND CONSTRUCTION SHALL CONFORM TO THE CITY'S STANDARDS AND SPECIFICATIONS AND NCTCOG 3RD EDITION.
  - IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT ALL PUBLIC UTILITIES IN THE CONSTRUCTION OF THIS PROJECT. ALL MANHOLES, CLEANOUTS, VALVE BOXES, FIRE HYDRANTS, ETC. MUST BE ADJUSTED TO PROPER LINE AND GRADE BY THE CONTRACTOR PRIOR TO AND AFTER THE PLACING OF PERMANENT PAVING. UTILITIES MUST BE MAINTAINED TO PROPER LINE AND GRADE DURING CONSTRUCTION OF THIS PROJECT.
  - THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH ALL THE APPROPRIATE UTILITY COMPANIES FOR THE LOCATION OF ALL UTILITIES WITHIN THE CONSTRUCTION AREA. CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES 48 HRS PRIOR TO ANY EXCAVATIONS. DIG TESS (1-800-545-6005)
  - THE PAVING CONTRACTOR SHALL COORDINATE WITH THE UTILITY CONTRACTOR TO INSURE ALL CONDUIT FOR IRRIGATION HAS BEEN INSTALLED PRIOR TO PLACEMENT OF PERMANENT PAVEMENT.
  - ALL EARTHWORK OPERATIONS, PAVEMENT AND BUILDING SUBGRADE PREPARATION SHALL COMPLY WITH ALL RECOMMENDATIONS CONTAINED IN THE GEOTECHNICAL REPORT, PROVIDED BY OTHERS.
  - TRAFFIC BARRICADES WILL BE REQUIRED FOR PAVING AND UTILITY CONSTRUCTION WITHIN THE PUBLIC RIGHT-OF-WAY. BARRICADES SHALL CONFORM TO THE INSTALLATION SHOWN IN THE LATEST TEXAS MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), AS CURRENTLY AMENDED BY THE TEXAS DEPARTMENT OF TRANSPORTATION. CONTACT CITY TRANSPORTATION DEPARTMENT PRIOR TO SETTING BARRICADES IN THE RIGHT OF WAY.
  - ALL DRIVE AREAS AND ISLANDS SHALL HAVE 6" CONCRETE CURB, UNLESS OTHERWISE NOTED ON PLANS.
  - CONCRETE PAVING SHALL HAVE A CONSTRUCTION JOINT OR SAWED CONTROL JOINT EVERY 15 FEET TRANSVERSELY AND LONGITUDINALLY WITH EXPANSION JOINTS AS SHOWN IN THE DRAWINGS. JOINTS SHALL INTERSECT ALL PAVEMENT EDGES AT 90° INCLUDING RADIUS RETURNS. EXPANSION JOINTS SHALL BE PLACED AT A MAXIMUM SPACING OF 100' CENTER TO CENTER. ALL JOINTS SHALL BE SEALED WITH HOT POURED RUBBER JOINT SEALING COMPOUND.
  - CONTRACTOR SHALL INSTALL ALL CONDUITS PRIOR TO THE PLACEMENT OF PAVEMENT. ALL SLEEVES SHOWN ON PLANS SHALL BE SCHEDULE 40 PVC WITH PULLED WIRES & CAPPED. REFER TO LANDSCAPE & MECHANICAL PLANS FOR LOCATIONS AND SIZE OF CONDUITS.
  - FIRE LANE PAVEMENT MARKINGS SHALL BE PAINTED LINES OF RED TRAFFIC PAINT, 6" WIDE, TO SHOW THE EXACT BOUNDARY LINES OF THE FIRE LANE. THESE BOUNDARY LINES MAY BE ON THE CURB OR FLAT PAVEMENT. THE LINES SHALL BE MARKED BY PAINTED 4" HIGH LETTERING USING A 1" WIDE STROKE OF WHITE TRAFFIC PAINT ON THE CONTRASTING RED BACKGROUND STATING "NO PARKING - FIRE LANE". THIS MARKING SHALL BE PLACED AT 25' INTERVALS ALONG EACH BOUNDARY LINE - SEE DIMENSIONAL CONTROL PLAN FOR DETAILS AND LOCATION.



ALL CURB WITHIN TxDOT RIGHT-OF-WAY MUST MEET TxDOT DETAIL SHEET CCCG-12

ADJUST EXISTING WATER VALVE COVER TO PROPOSED GRADE

PROPOSED RETAINING WALL TO BE DESIGNED BY OTHERS

TEMP. BENCHMARK "X" CUT IN CONCRETE CURB ELEV. = 556.25

**LEGEND**

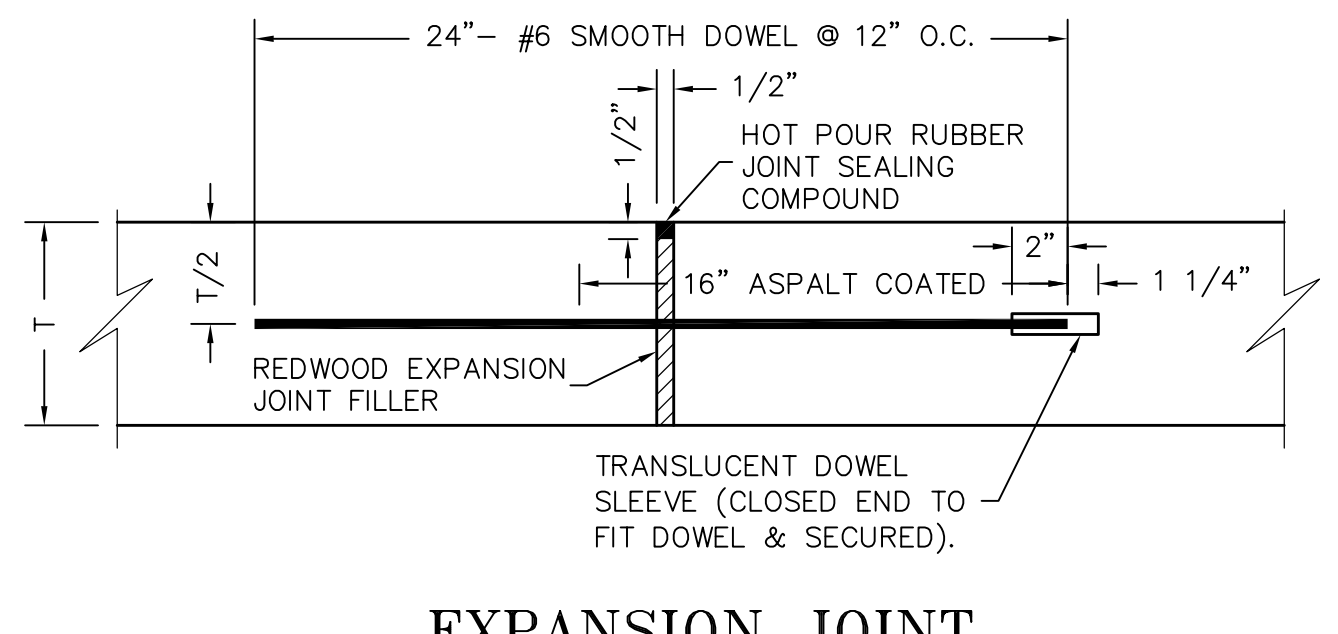
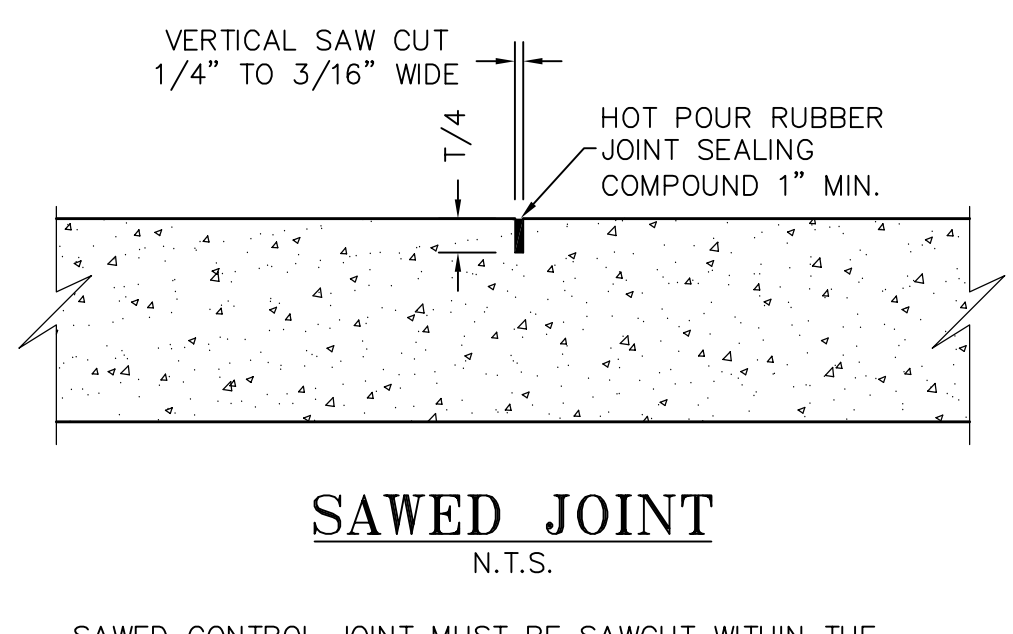
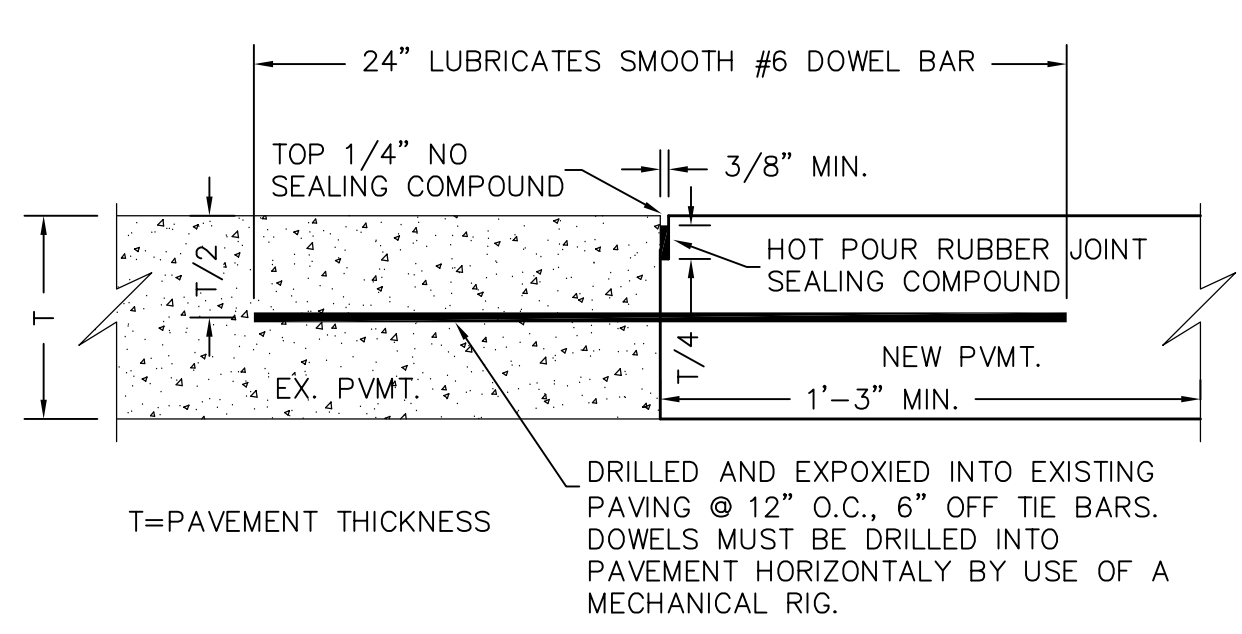
- 6" REINF. CONC. PAVEMENT (6.5 SACK-3600 psi) W/ #3 BARS @ 18" O.C.E.W. OVER 6" SUBGRADE, SCARIFIED AND COMPACTED TO 95% STD. PROCTOR DENSITY (ASTM-D698). W/ +2% to +4% OPTIMUM MOISTURE CONTENT. (MAX. P.I. LESS THAN 15) (DRIVE) - NO SAND CUSHING UNDER PAVING
- 5" REINF. CONC. PAVEMENT (6.5 SACK-3600 psi) W/ #3 BARS @ 18" O.C.E.W. OVER 6" SUBGRADE, SCARIFIED AND COMPACTED TO 95% STD. PROCTOR DENSITY (ASTM-D698). W/ +2% to +4% OPTIMUM MOISTURE CONTENT. (MAX. P.I. LESS THAN 15) (PARKING) - NO SAND CUSHING UNDER PAVING
- SJ SAWED CONTRACTION JOINT
- EJ EXPANSION JOINT

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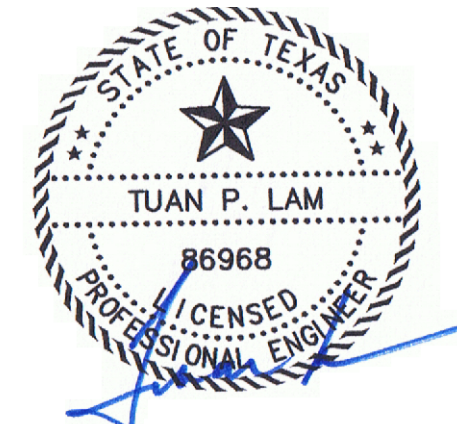
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SAWED CONTROL JOINT MUST BE SAWCUT WITHIN THE FIRST 24 HOURS AFTER THE CONCRETE PAVEMENT IS POURED. FAILURE TO DO SO MAY CAUSE UNCONTROLLED CRACKS AND WILL RESULT IN REMOVAL AND REPLACEMENT OF CONCRETE AT THE CONTRACTOR COSTS.



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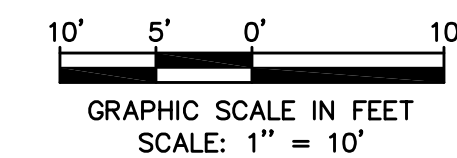
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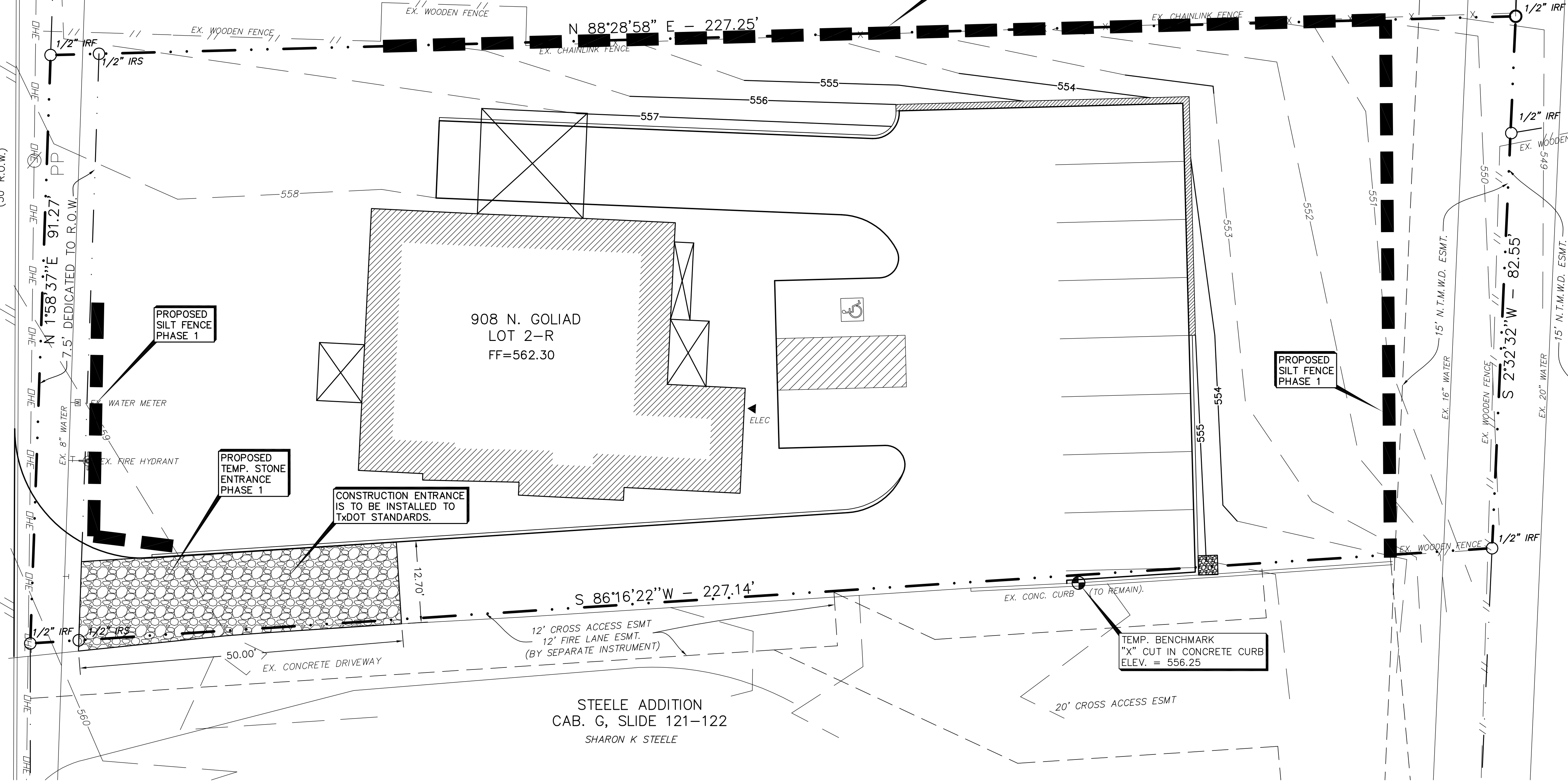
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JANET C WINTERS REUCK & LEE REUCK  
TO  
MARY A STACEY  
VOL. 763, PG. 13



- GENERAL NOTES:**
1. EROSION CONTROL DEVICES AS SHOWN ON THE EROSION CONTROL PLAN FOR THE PROJECT SHALL BE INSTALLED PRIOR TO THE START OF LAND DISTURBING ACTIVITIES ON THE PROJECT.
  2. ALL EROSION CONTROL DEVICES ARE TO BE INSTALLED IN ACCORDANCE WITH THE APPROVED PLANS AND SPECIFICATIONS FOR THE PROJECT. CHANGES ARE TO BE APPROVED BEFORE CONSTRUCTION BY THE DESIGN ENGINEER AND THE CITY ENGINEERING DIVISION.
  3. IF THE EROSION CONTROL PLAN AS APPROVED CANNOT CONTROL EROSION AND OFF-SITE SEDIMENTATION FROM THE PROJECT THE EROSION CONTROL PLAN WILL BE REQUIRED TO BE REVISED AND/OR ADDITIONAL EROSION CONTROL DEVICES WILL BE REQUIRED ON SITE.
  4. IF OFF-SITE SOIL BORROW OR SPOIL SITES ARE USED IN CONJUNCTION WITH THIS PROJECT, THIS INFORMATION SHALL BE DISCLOSED AND SHOWN ON THE EROSION CONTROL PLAN. OFF-SITE BORROW AND SPOIL AREAS ARE CONSIDERED A PART OF THE PROJECT SITE AND THEREFORE SHALL COMPLY WITH THE CITY'S EROSION CONTROL REQUIREMENTS. THESE AREAS SHALL BE STABILIZED WITH PERMANENT GROUND COVER PRIOR TO FINAL APPROVAL OF THE PROJECT.
  5. EROSION CONTROL MEASURES SHALL BE INSPECTED AND REPAIRED, IF NECESSARY, AT THE EARLIEST POSSIBLE DATE BUT NO LATER THAN SEVEN (7) CALENDAR DAYS AFTER EACH RAIN. ANY ITEM DISTURBED BY THE CONTRACTOR SHALL BE REPAIRED.
  6. THE CONTRACTOR IS RESPONSIBLE FOR MONITORING DOWNSTREAM CONDITIONS THROUGHOUT THE CONSTRUCTION PERIOD AND WILL CLEAN ANY DEBRIS AND SEDIMENT CAUSED BY CONSTRUCTION
  7. THE CONTRACTOR SHALL PREVENT EROSION OF THE SITE AND PROTECT ALL DRAINAGE STRUCTURES BY THE USE OF SILT FENCING OR OTHER APPROVED EROSION CONTROL PRODUCTS AS NEEDED. TEMPORARY EROSION CONTROL DEVICES INSTALLED BY THE CONTRACTOR ON THIS PROJECT SHALL ALSO BE REMOVED BY THE CONTRACTOR AT THE APPROPRIATE TIME AS INCIDENTAL TO THE RESPECTIVE EROSION CONTROL DEVICE ITEM.
  8. ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED IF DEEMED NECESSARY BY THE PROJECT ENGINEER DURING ON-SITE INSPECTIONS.
  9. WETLANDS ARE NOT PRESENT ON-SITE NOR ADJACENT TO THIS PROJECT.
  10. ALL POLLUTION PREVENTION CONTROL DEVICES SHALL CONFORM TO THE CITY'S EROSION AND SEDIMENT CONTROL MANUAL.
  11. IF THE EROSION CONTROL PLAN AS APPROVED CANNOT CONTROL EROSION AND OFF-SITE SEDIMENTATION FROM THE PROJECT THE EROSION CONTROL PLAN WILL BE REQUIRED TO BE REVISED AND/OR ADDITIONAL EROSION CONTROL DEVICES WILL BE REQUIRED ON SITE.
  12. CONTRACTOR SHALL ADD OR REMOVE EROSION CONTROL DEVICES AS NEED DURING THE CONSTRUCTION PHASES.



PROPOSED SILT FENCE PHASE 1

PROPOSED TEMP. STONE ENTRANCE PHASE 1

CONSTRUCTION ENTRANCE IS TO BE INSTALLED TO TxDOT STANDARDS.

PROPOSED SILT FENCE PHASE 1

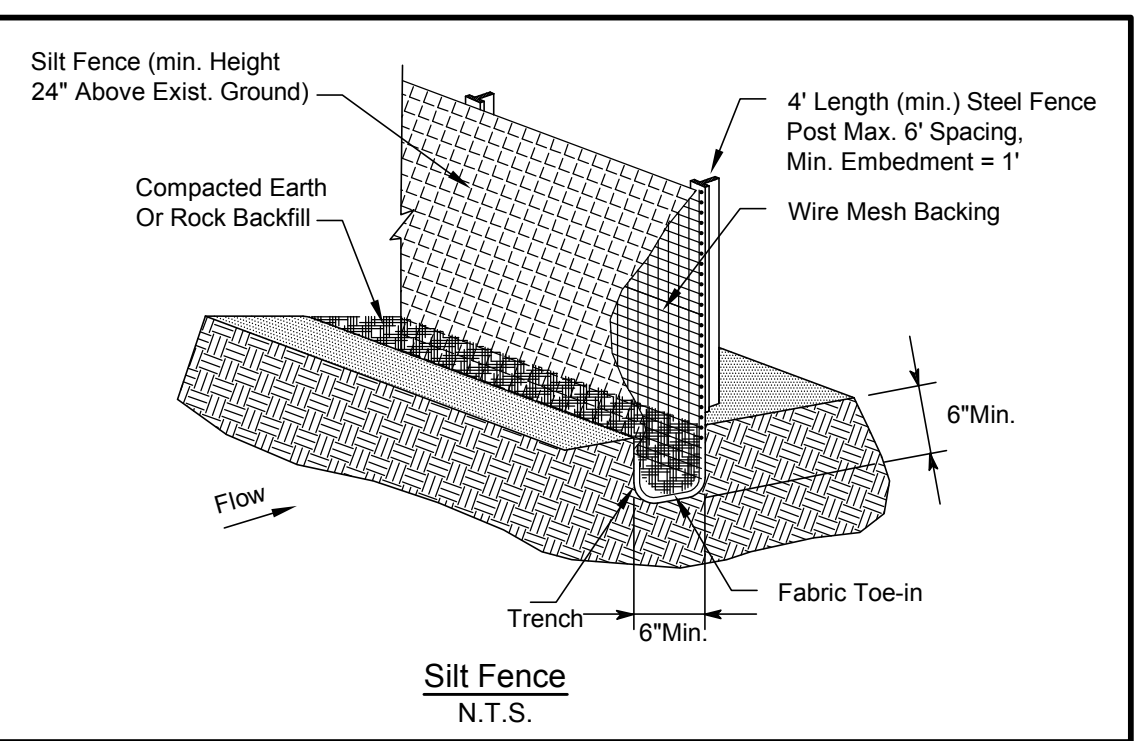
TEMP. BENCHMARK "X" CUT IN CONCRETE CURB ELEV. = 556.25

**LEGEND**

- SILT FENCE
- TEMP. STONE ENTRANCE

**AS-BUILT PLANS**

TO THE BEST OF OUR KNOWLEDGE LAM CONSULTING ENGINEERING, HEREBY STATES THAT THIS PLAN IS AS-BUILT. THIS INFORMATION PROVIDED IS BASED ON THE OWNER SURVEYOR SURVEYING AT THE SITE. 04/29/13



**CONSTRUCTION SCHEDULE & PHASING**

- PHASE 1: INSTALL SILT FENCE AND STONE CONSTRUCTION ENTRANCE FOR ROUGH GRADING.
- PHASE 2: PAVE PARKING LOT AND DRIVEWAY.

**NOTE:**  
CONTRACTOR SHALL PROVIDE CONSTRUCTION SITE NOTICE TO THE CITY PRIOR TO START OF CONSTRUCTION.

CONTRACTOR SHALL PAY ALL PERMIT FEES AND WILL SETUP PRE-CONSTRUCTION MEETING PRIOR TO COMMENCING ANY WORK.

**B.M.P. MAINT. SCHED.**

BMP	MAINT. FREQ.	BY:

THE INFORMATION SHOWN ON THIS SITE PLAN ARE FROM RECORD DRAWINGS. THE ENGINEER IS NOT RESPONSIBLE FOR THE ACCURACY OF THE INFORMATION SHOWN ON THIS DRAWING. CONTRACTOR SHALL FIELD VERIFY ALL UTILITIES AND CONTACT ALL UTILITY COMPANIES 48 HRS PRIOR TO ANY EXCAVATION. 1-800-DIG TESS



THE SEAL APPEARING ON THIS DOCUMENT WAS AUTHORIZED BY TUAN P. LAM, P.E. 86968 ON 03/18/13. ALTERATION OF A SEALED DOCUMENT WITHOUT THE PROPER NOTIFICATION TO THE RESPONSIBLE ENGINEER IS AN OFFENSE UNDER THE TEXAS ENGINEERING PRACTICE ACT.

**EROSION CONTROL PLAN**

**W.D. AUSTIN ADDITION - LOT 2-R  
HEAVENLY HANDS BIRTHING CENTER  
908 N. GOLIAD STREET  
ROCKWALL, TEXAS 75087**

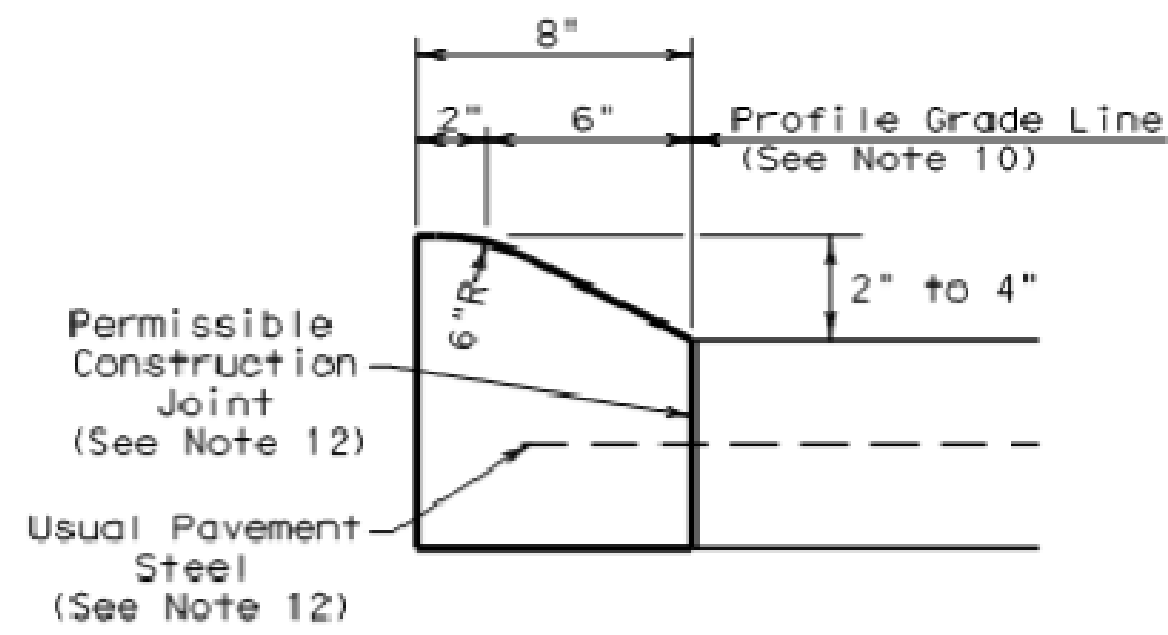
**LAM CONSULTING ENGINEERING**  
6804 WILHELMINA DRIVE  
SACHSE, TEXAS 75048  
Phone (214) 766-1011  
www.lamcivil.com  
Firm #F-9763

DESIGN: LCE	CHECK: LCE	SCALE: AS NOTED
DRAWN: CTL	DATE: JANUARY 2013	PROJECT: 0428-13

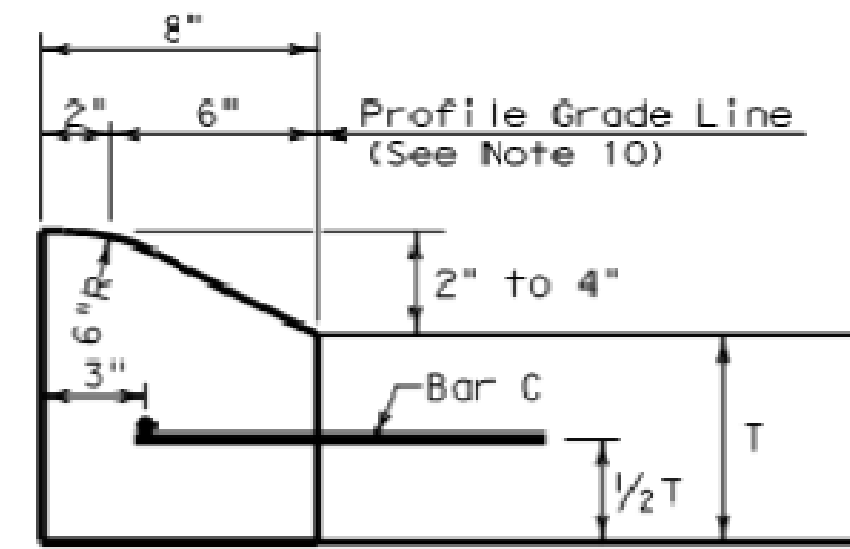
6 of 6

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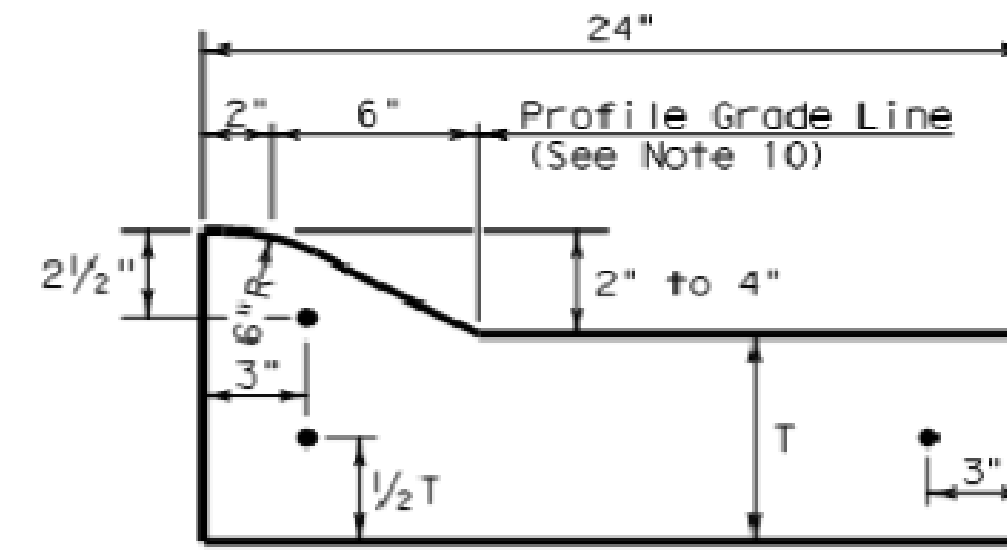
DATE: FILE:



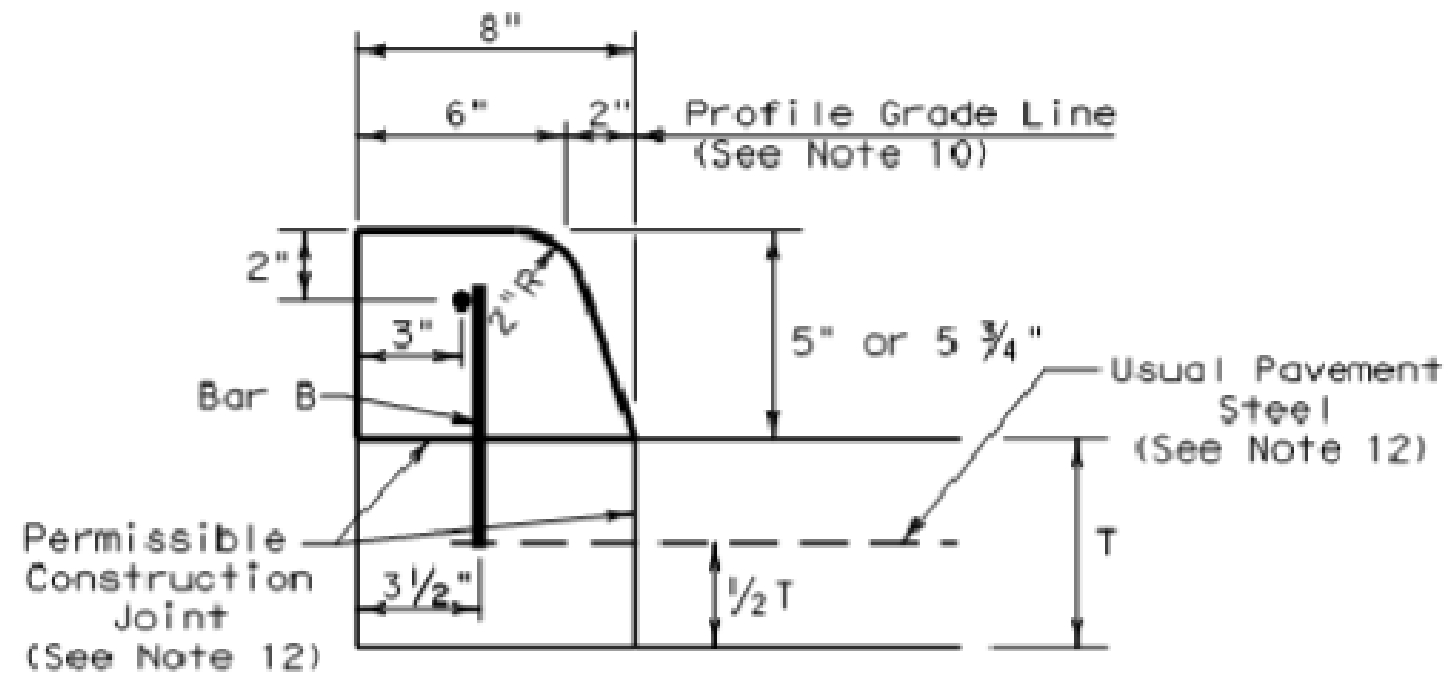
**TYPE I CURB (MONOLITHIC)**  
2" - 4" HEIGHT



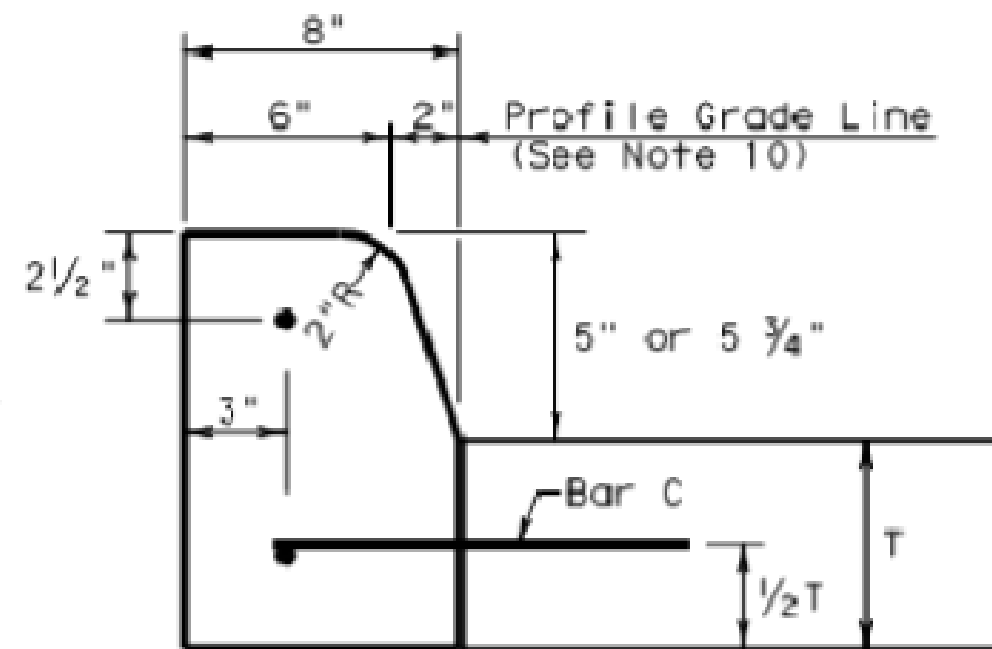
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2" - 4" HEIGHT



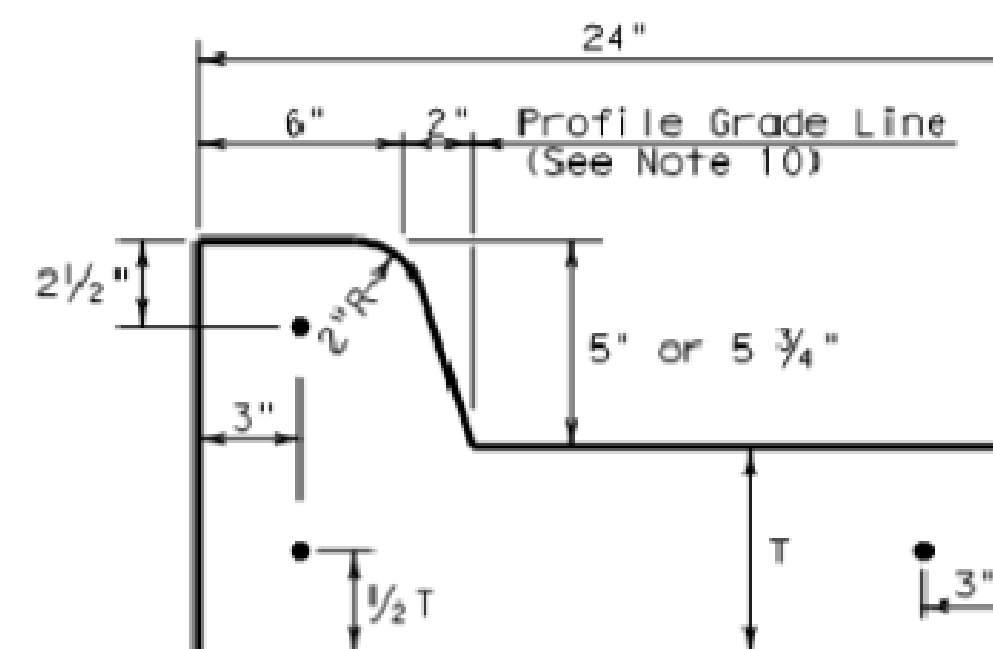
**TYPE I CURB AND GUTTER**  
2" - 4" HEIGHT



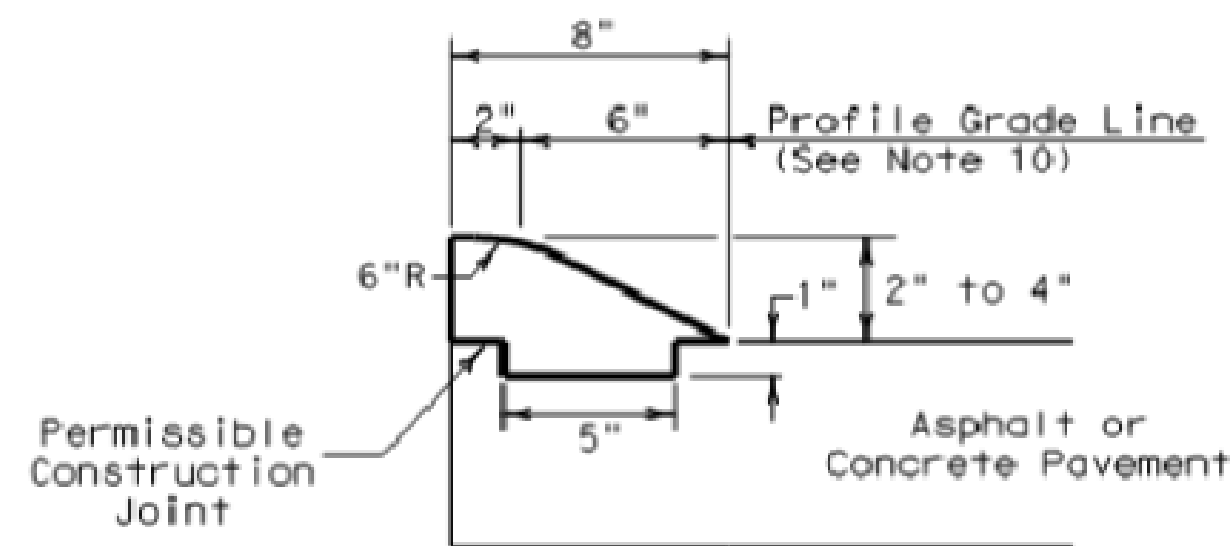
**TYPE II CURB (MONOLITHIC)**  
5" - 5 3/4" HEIGHT



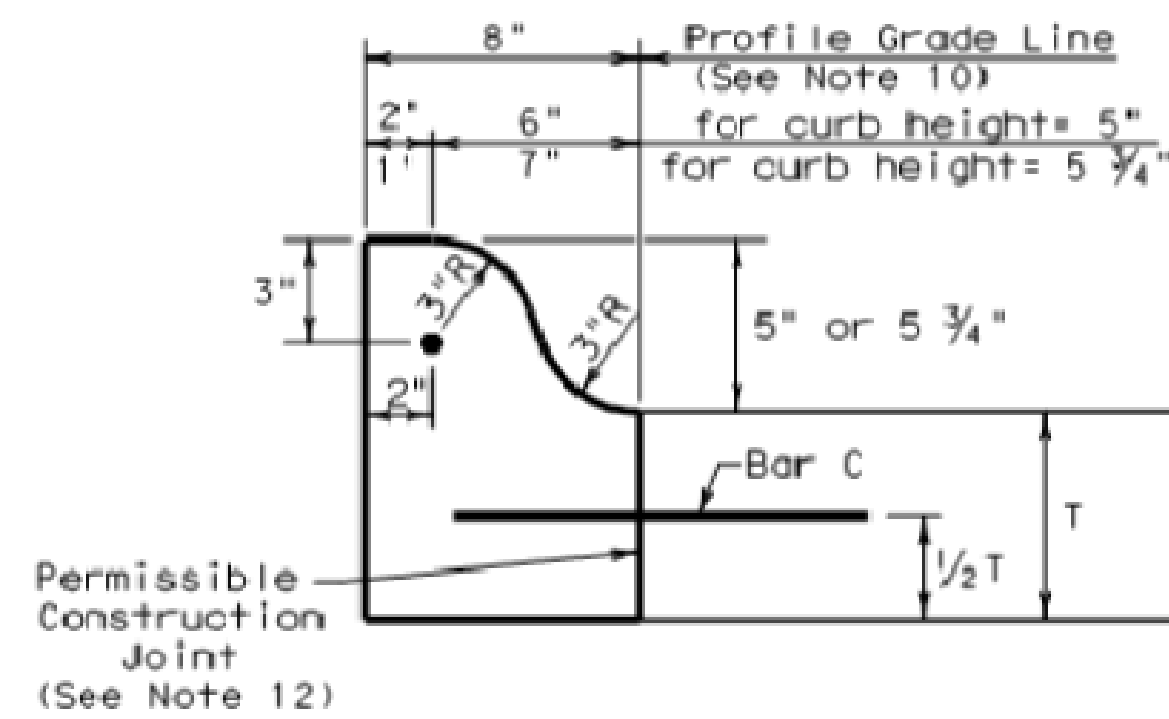
**TYPE II CURB**  
5" - 5 3/4" HEIGHT



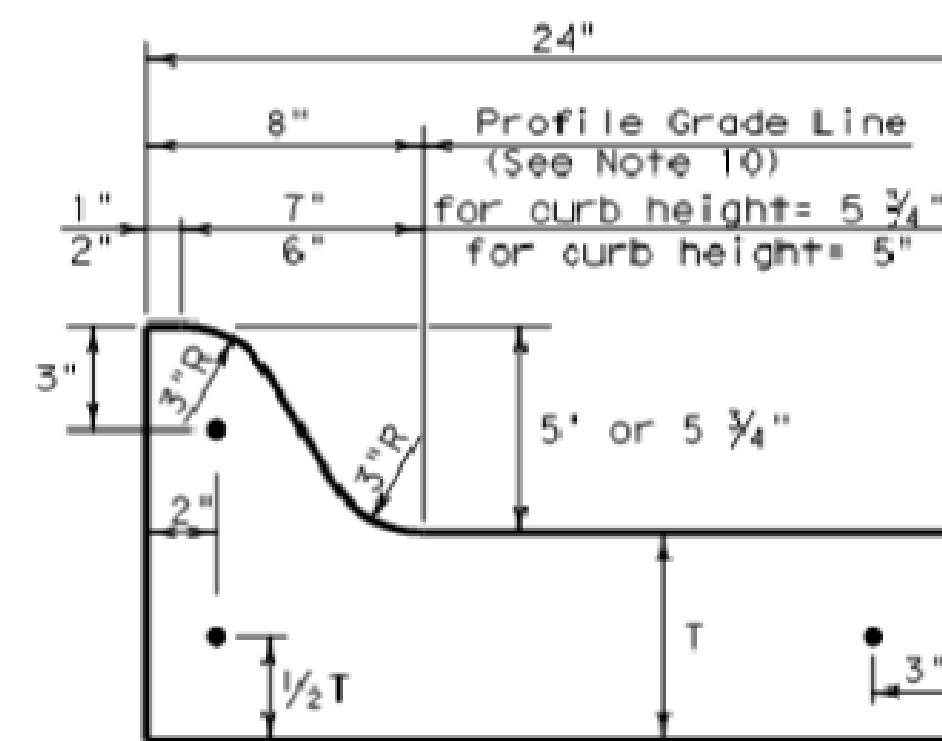
**TYPE II CURB AND GUTTER**  
5" - 5 3/4" HEIGHT



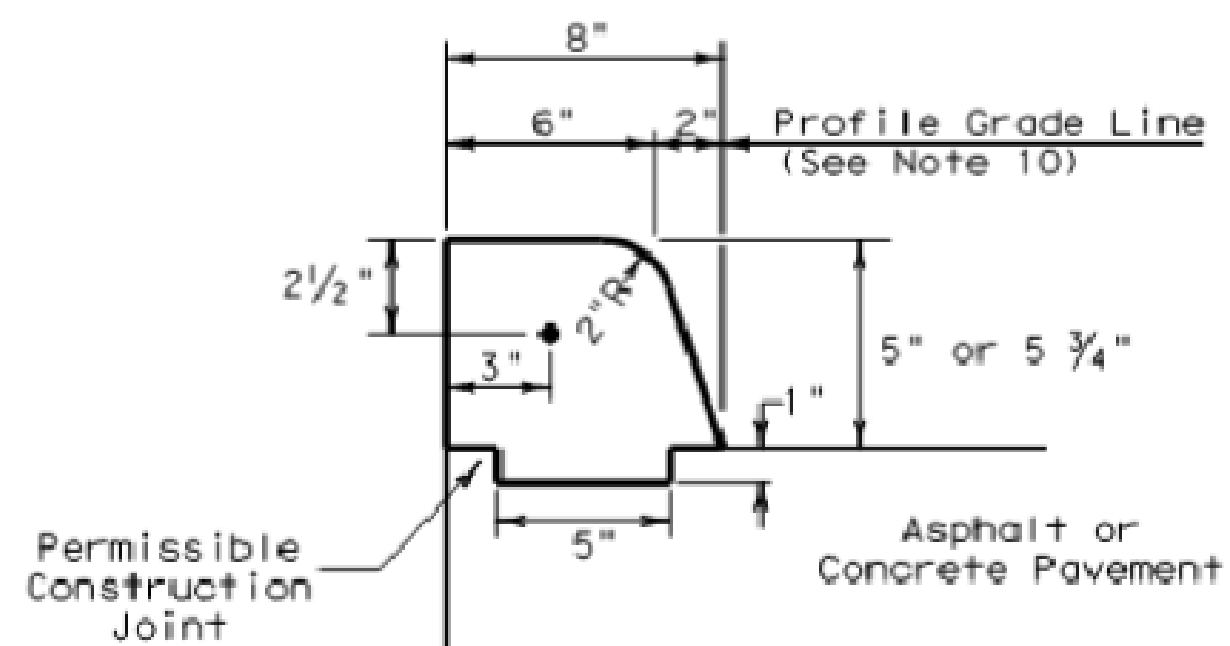
**TYPE III CURB (KEYED)**  
2" - 4" HEIGHT



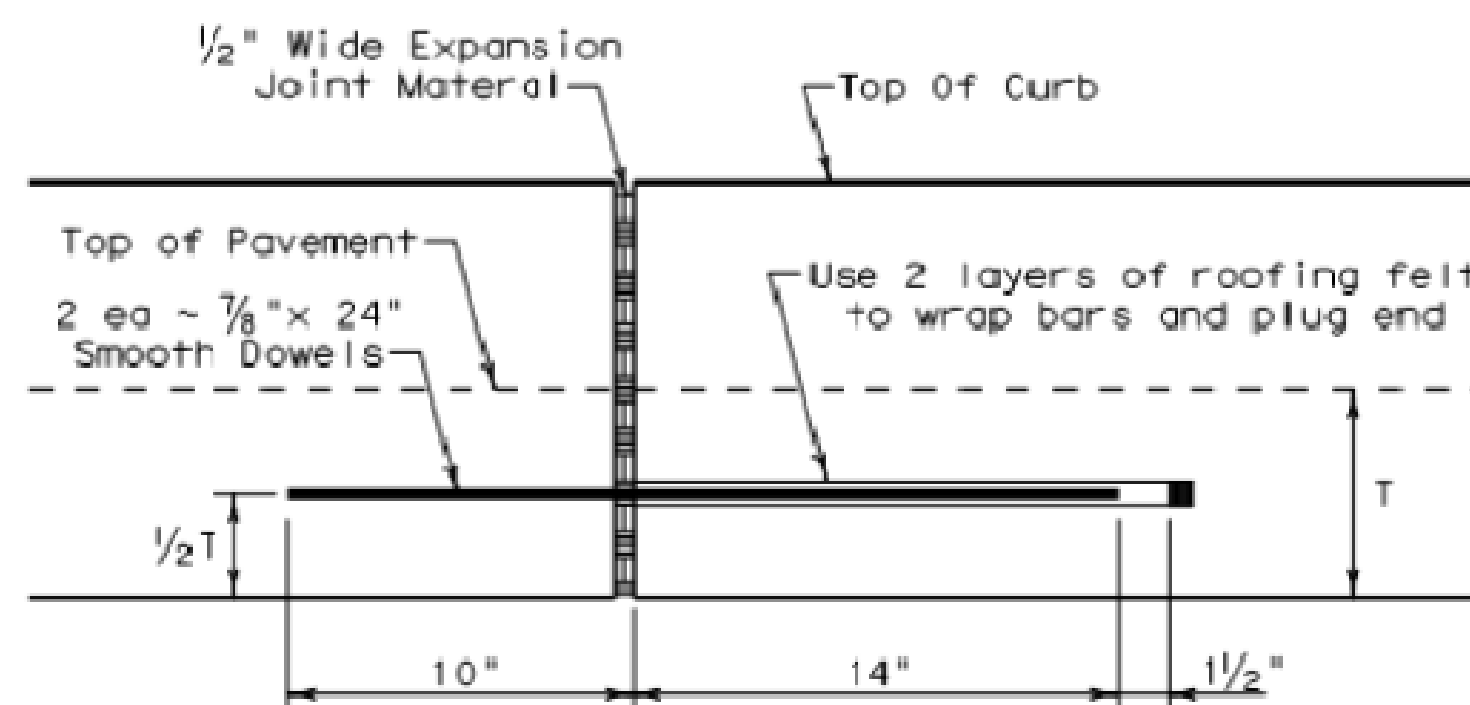
**TYPE IIa CURB**  
5" - 5 3/4" HEIGHT



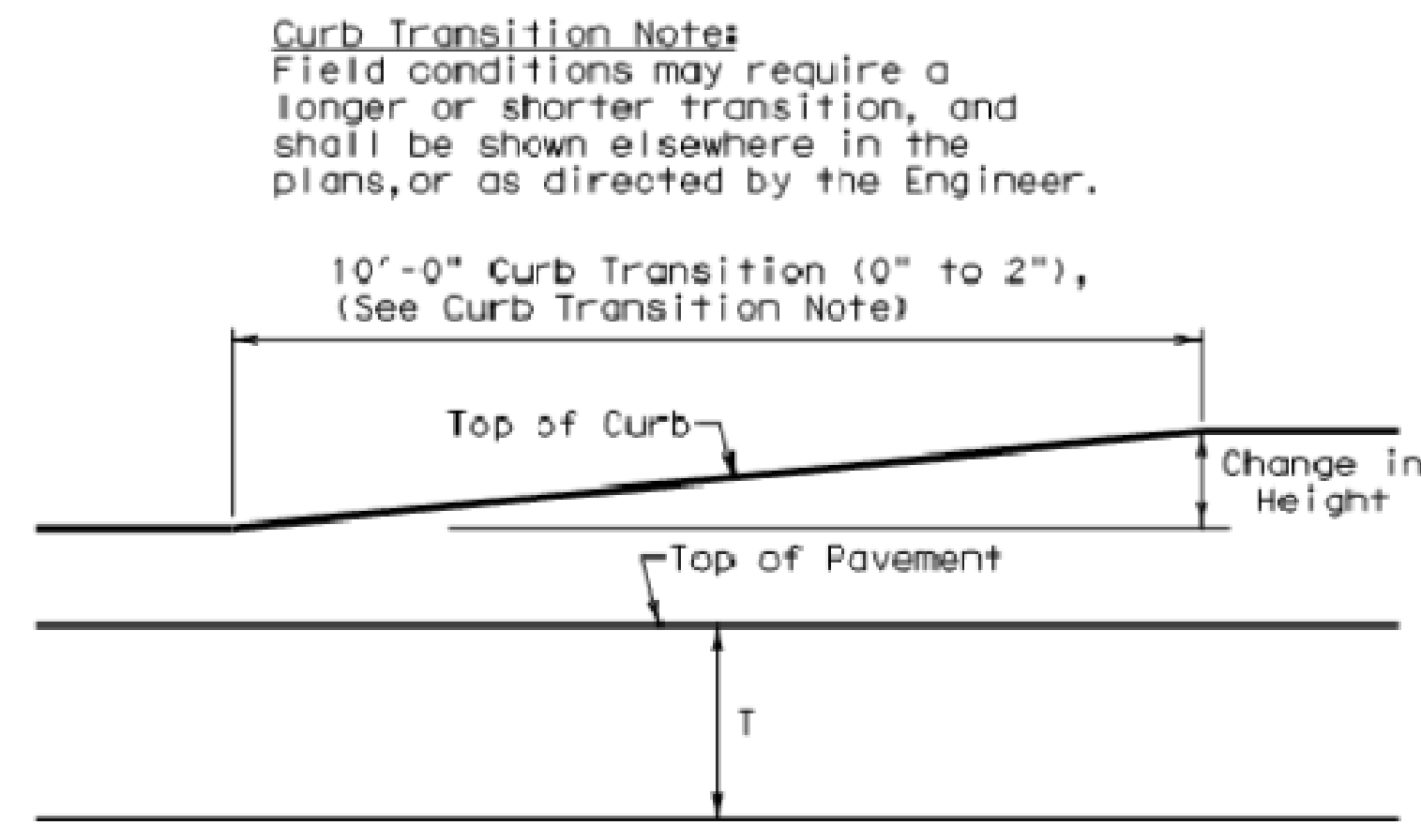
**TYPE IIa CURB AND GUTTER**  
5" - 5 3/4" HEIGHT



**TYPE IV CURB (KEYED)**  
5" - 5 3/4" HEIGHT



**EXPANSION JOINT DETAIL**

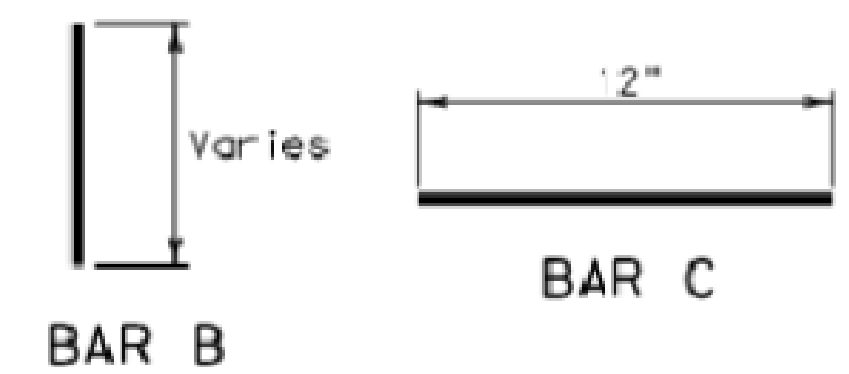


**CURB TRANSITION**

Note: To be paid for as Highest Curb

**General Notes**

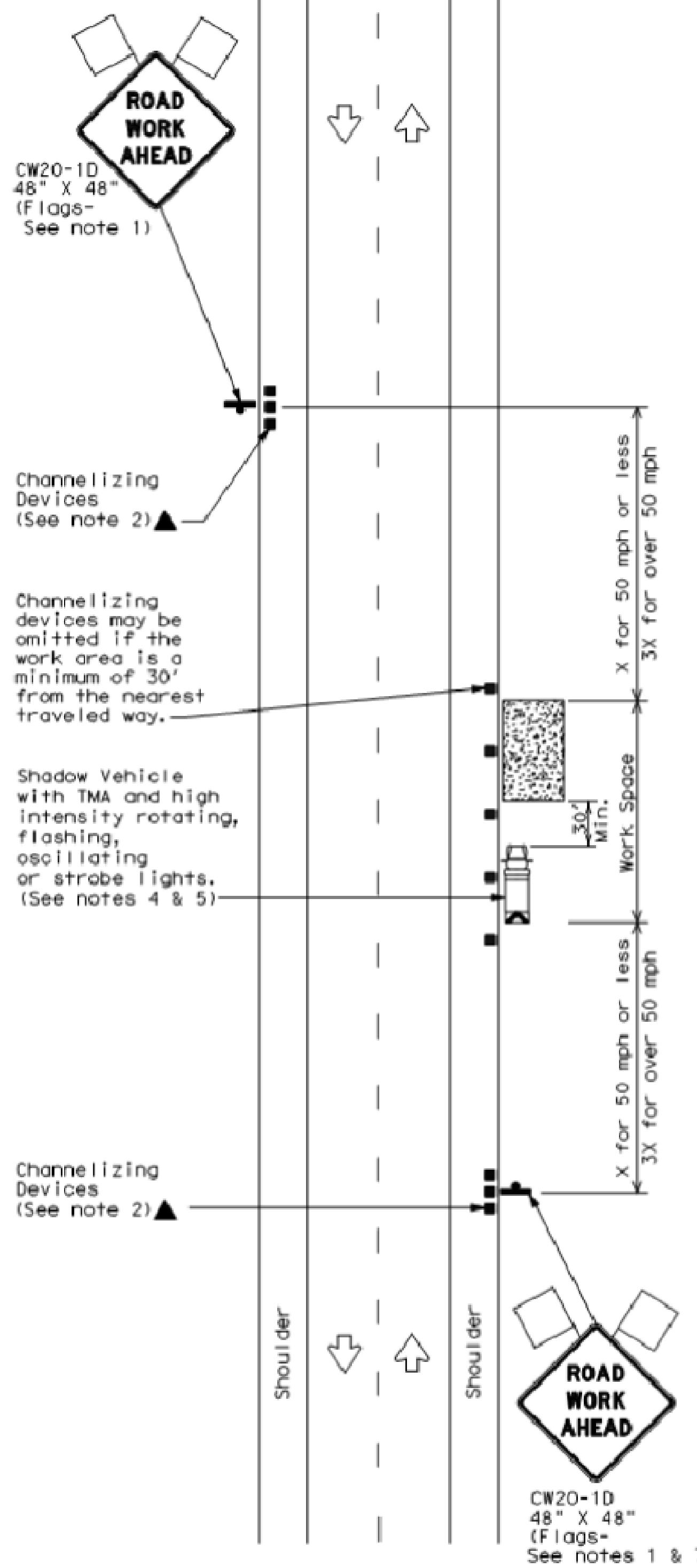
1. All materials and construction shall be in accordance with Item 529, "Concrete Curb, Gutter, and Combined Curb and Gutter."
2. Concrete shall be Class A.
3. When reinforcing bars are used, they shall be No.4 unless otherwise shown. The use of synthetic fiber in lieu of steel reinforcing is acceptable, provided the fiber producer is on the Department Producer List (MPL), maintained by TxDOT, Construction Division.
4. Round exposed sharp edges with a rounding tool, to a minimum radius of 1/4 inch.
5. All existing curbs and driveways to be removed shall be sawed or removed at existing joints.
6. Where concrete curb is placed on existing concrete pavement, the pavement shall be drilled and the reinforcing bars grouted in place.
7. Expansion and contraction joints shall be constructed to match pavement joints in all curbs and curb and gutter adjacent to jointed concrete pavement. Where placement of curb or curb and gutter is not adjacent to concrete pavement, expansion joints shall be provided at structures, curb returns at streets, and at locations directed by The Engineer.
8. Vertical and horizontal dowel bars and transverse reinforcing bars shall be placed at four feet C-C.
9. Dimension 'T' shown is the thickness of concrete pavement. When curb is installed adjacent to flexible pavement dimension 'T' is 8" maximum.
10. Usual profile grade line. Refer to typical sections and plan-profile sheets for exact locations.
11. One-half inch expansion joint material shall be provided where curb or curb and gutter is adjacent to sidewalk or riprap.
12. When vertical permissible construction joints are used, resulting in a longitudinal construction joint in the pavement, the longitudinal pavement steel shall be placed in accordance with pavement details shown elsewhere in the plans for longitudinal construction joints. Reinforcing steel for curb section shall then conform to that required for concrete curb.



**Curb Transition Note:**  
Field conditions may require a longer or shorter transition, and shall be shown elsewhere in the plans, or as directed by the Engineer.

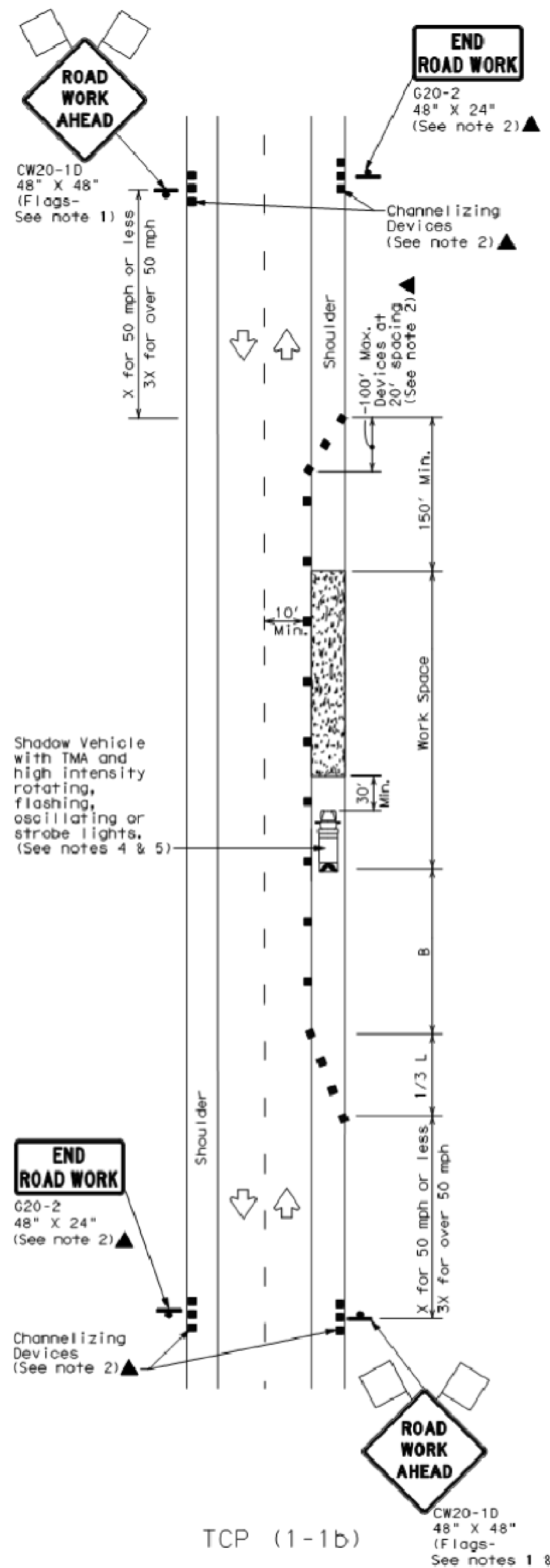
		<b>Design Division Standard</b>	
<h2>CONCRETE CURB AND GUTTER</h2> <h3>CCCG-12</h3>			
FILE: cccg12	DATE: TxDOT	CHK: AM	DWG: VP
© TxDOT 1995	CONT: SECT	JOB:	HIGHWAY:
REVISIONS		DIST:	COUNTY:
		SHEET NO.:	

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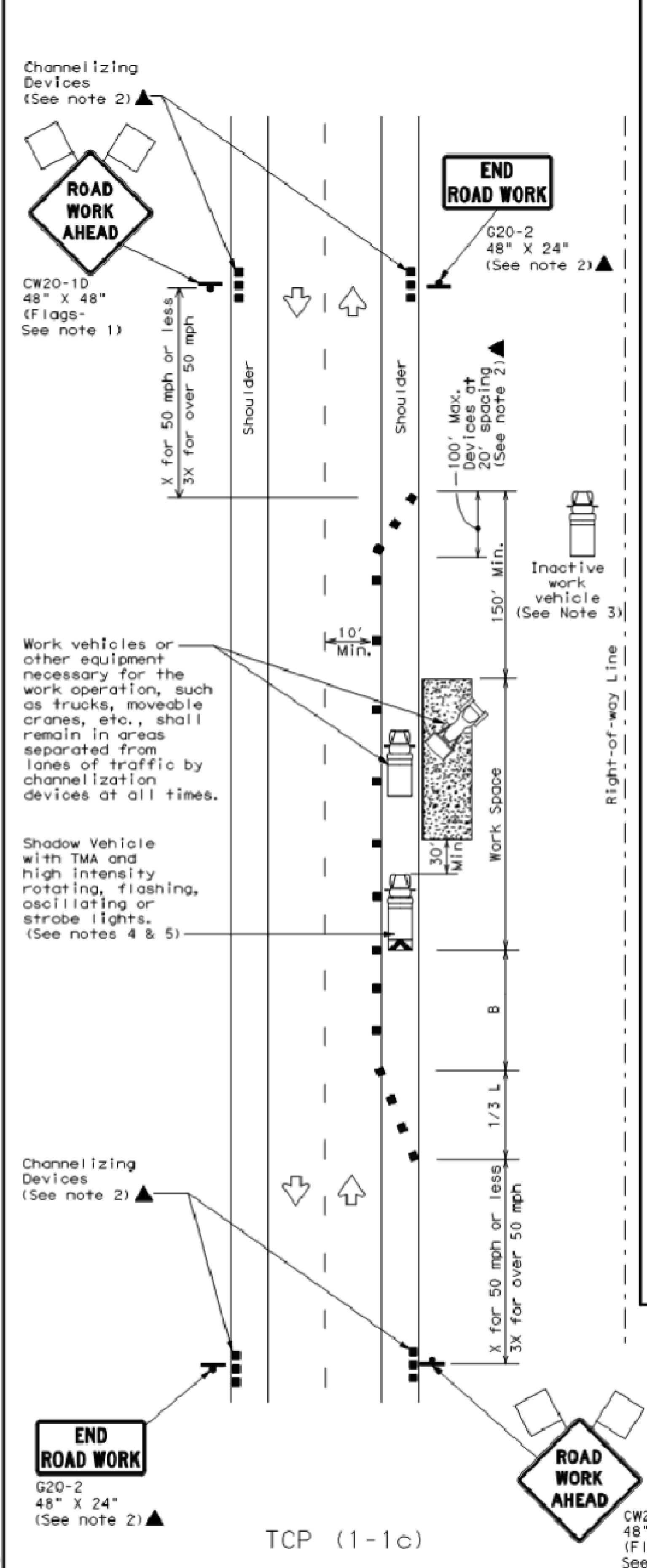
TCP (1-1a)

**WORK SPACE NEAR SHOULDER**  
Conventional Roads



TCP (1-1b)

**WORK SPACE ON SHOULDER**  
Conventional Roads



TCP (1-1c)

**WORK VEHICLES ON SHOULDER**  
Conventional Roads

**LEGEND**

	Type 3 Barricade		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posted Speed *	Formula	Minimum Desirable Taper Lengths **			Suggested Maximum Spacing of Channelizing Devices		Minimum Sign Spacing "X" Distance	Suggested Longitudinal Buffer Space "B"
		10' Offset	11' Offset	12' Offset	On a Taper	On a Tangent		
30	L = WS <sup>2</sup> / 60	150'	165'	180'	30'	60'	120'	90'
35		205'	225'	245'	35'	70'	160'	120'
40	L = WS	265'	295'	320'	40'	80'	240'	155'
45		450'	495'	540'	45'	90'	320'	195'
50	L = WS	500'	550'	600'	50'	100'	400'	240'
55		550'	605'	660'	55'	110'	500'	295'
60	L = WS	600'	660'	720'	60'	120'	600'	350'
65		650'	715'	780'	65'	130'	700'	410'
70	L = WS	700'	770'	840'	70'	140'	800'	475'
75		750'	825'	900'	75'	150'	900'	540'

\* Conventional Roads Only  
 \*\* Taper lengths have been rounded off.  
 L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

**TYPICAL USAGE**

	MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
		✓	✓		

- GENERAL NOTES**
- Flags attached to signs where shown are REQUIRED.
  - All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stated elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
  - Inactive work vehicles or other equipment should be parked near the right-of-way line and not parked on the paved shoulder.
  - A Shadow Vehicle with a TMA should be used anytime it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer present but road or work conditions require the traffic control to remain in place, Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TMA.
  - Additional Shadow Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect wider work spaces.
  - See TCP(5-1) for shoulder work on divided highways, expressways and freeways.
  - CW21-5 "SHOULDER WORK" signs may be used in place of CW20-1D "ROAD WORK AHEAD" signs for shoulder work on conventional roadways.

For construction or maintenance contract work, specific project requirements for shadow vehicles can be found in the project GENERAL NOTES for Item 502, Barricades, Signs and Traffic Handling.



**TRAFFIC CONTROL PLAN  
CONVENTIONAL ROAD  
SHOULDER WORK**

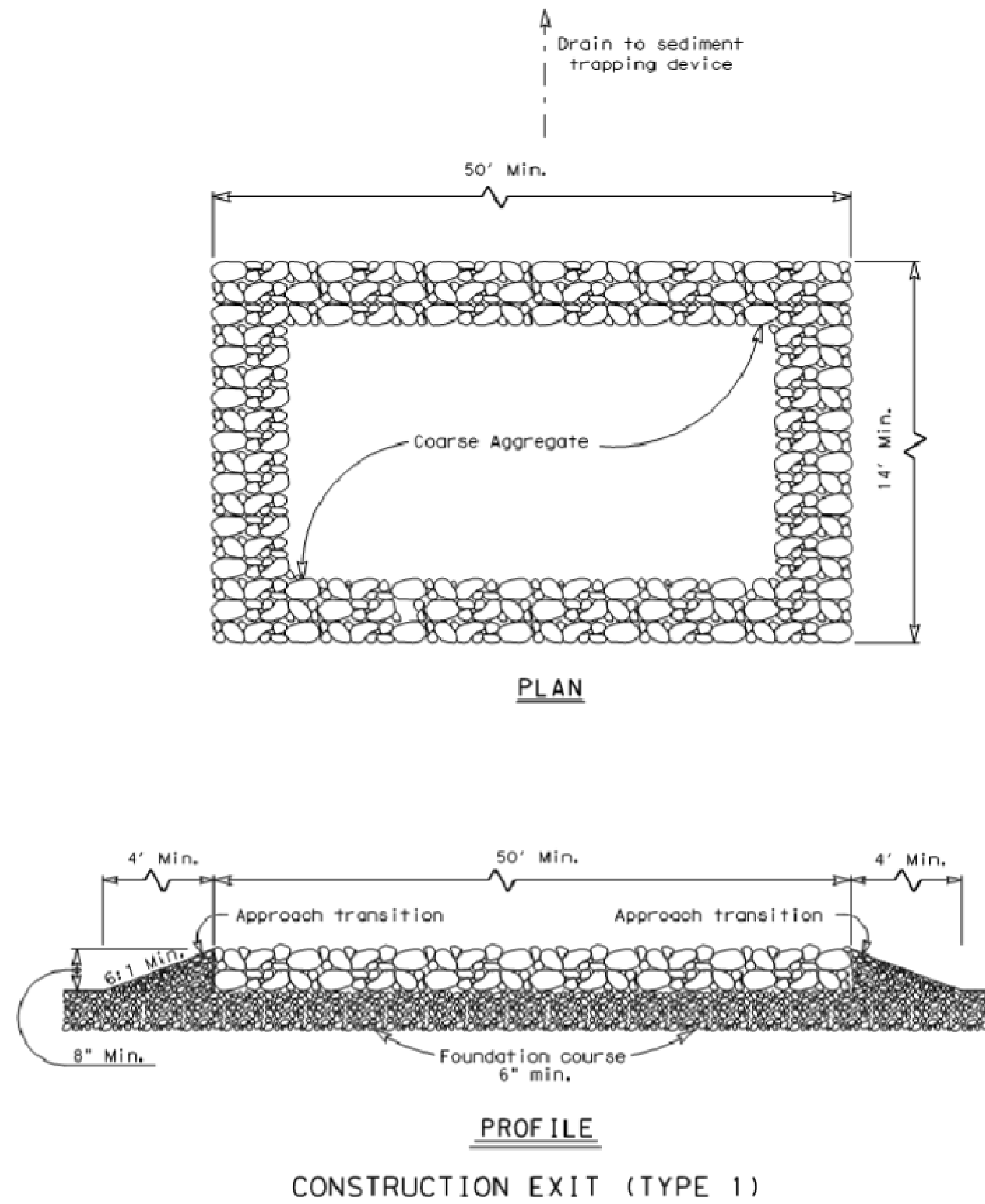
TCP(1-1)-12

© TxDOT December 1985		DN: TXDOT	CK: TXDOT	DN: TXDOT	CK: TXDOT
REVISIONS					
2-94	2-12	CONT	SECT	JOB	HIGHWAY
8-95					
1-97		DIST	COUNTY		SHEET NO.
4-98					

DATE: FILE:

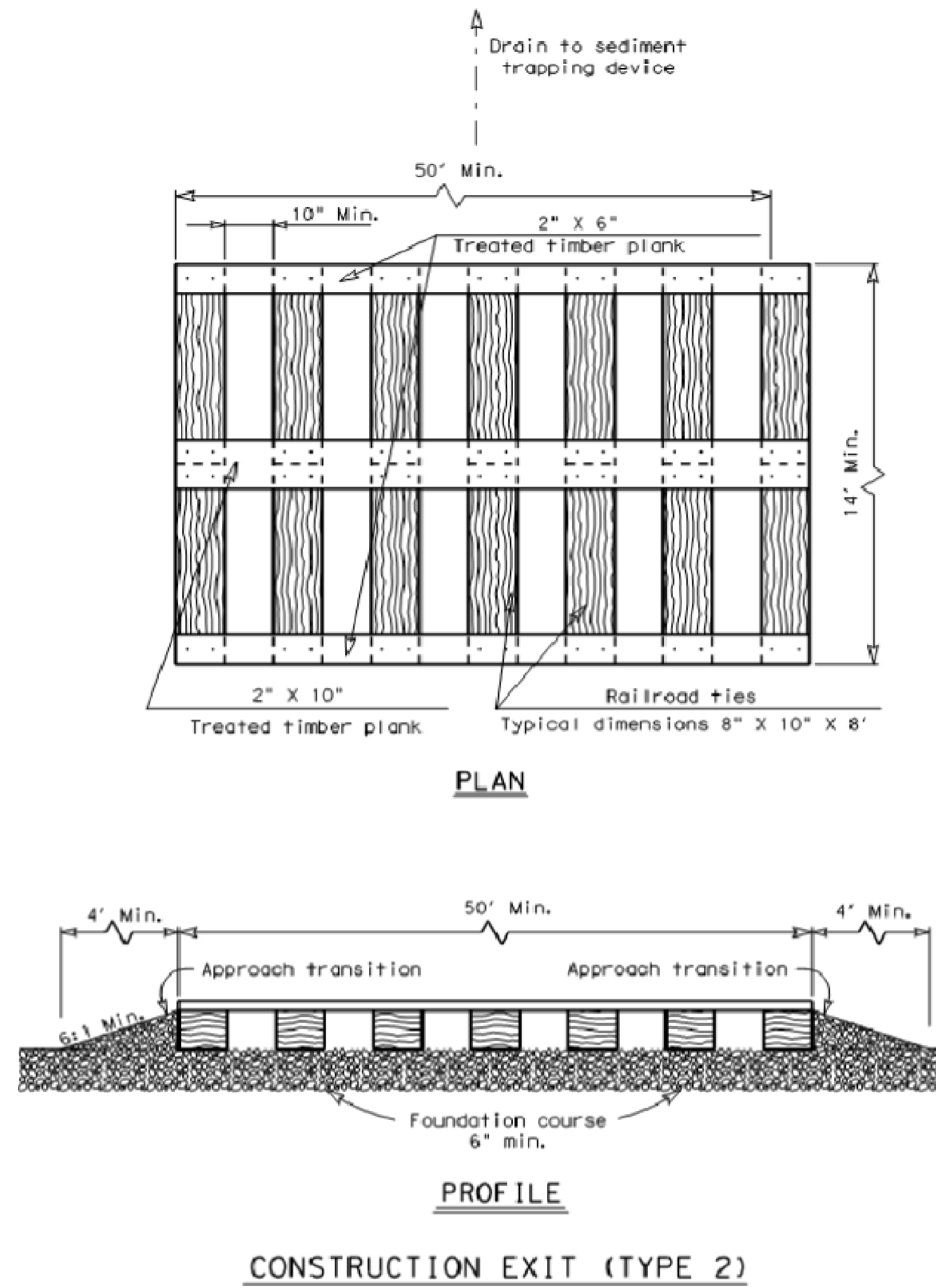


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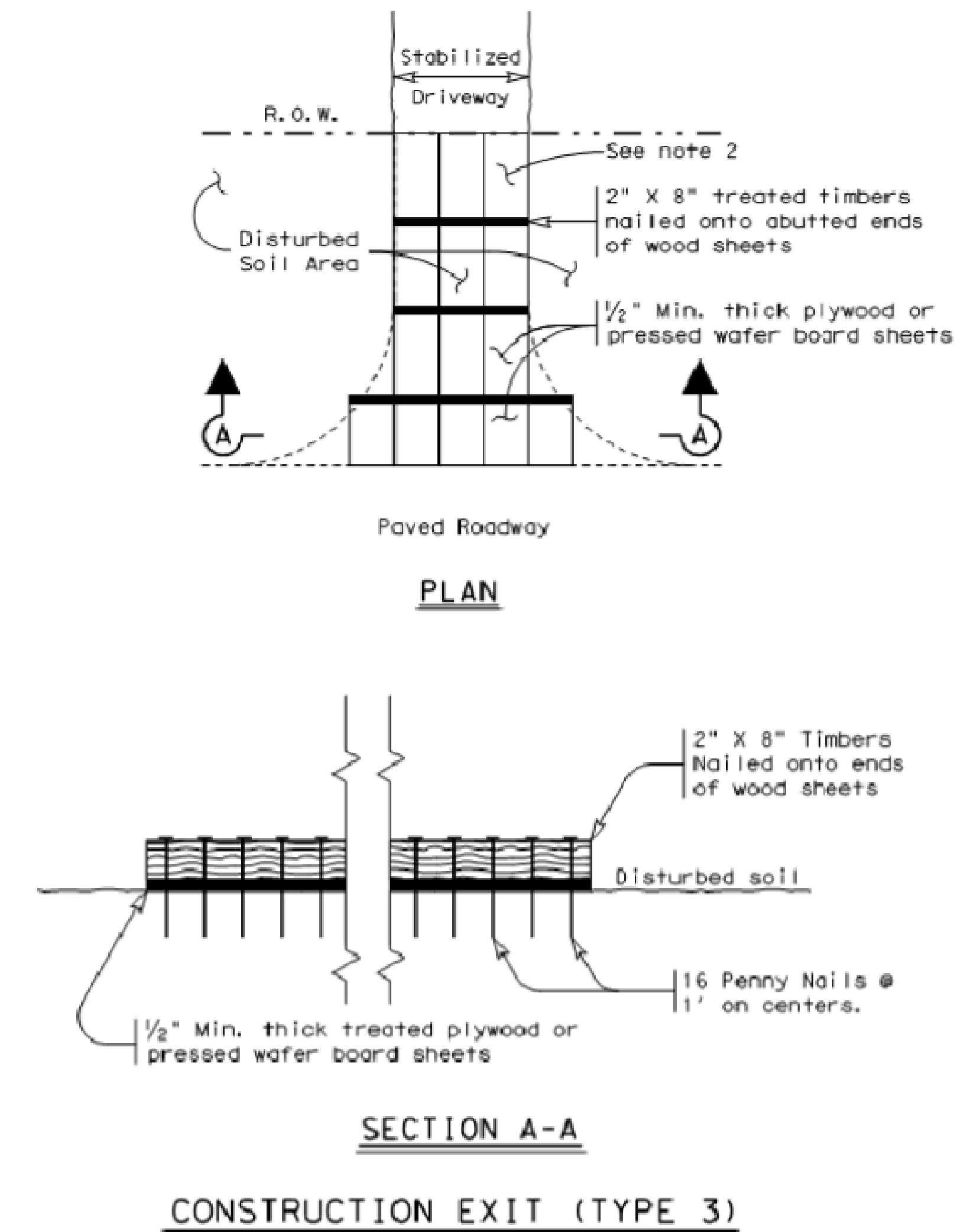
**GENERAL NOTES**

1. The length of the type 1 construction exit shall be as indicated on the plans, but not less than 50'.
2. The coarse aggregate should be open graded with a size of 4" to 8".
3. The approach transitions should be no steeper than 6:1 and constructed as directed by the Engineer.
4. The construction exit foundation course shall be flexible base, bituminous concrete, portland cement concrete or other material as approved by the Engineer.
5. The construction exit shall be graded to allow drainage to a sediment trapping device.
6. The guidelines shown hereon are suggestions only and may be modified by the Engineer.



**GENERAL NOTES**

1. The length of the type 2 construction exit shall be as indicated on the plans, but not less than 50'.
2. The treated timber planks shall be attached to the railroad ties with 1/2" x 6" min. lag bolts. Other fasteners may be used as approved by the Engineer.
3. The treated timber planks shall be #2 grade min., and should be free from large and loose knots.
4. The approach transitions shall be no steeper than 6:1 and constructed as directed by the Engineer.
5. The construction exit foundation course shall be flexible base, bituminous concrete, portland cement concrete or other material as approved by the Engineer.
6. The construction exit should be graded to allow drainage to a sediment trapping device.
7. The guidelines shown hereon are suggestions only and may be modified by the Engineer.



**GENERAL NOTES**

1. The length of the type 3 construction exit shall be as shown on the plans, or as directed by the Engineer.
2. The type 3 construction exit may be constructed from open graded crushed stone with a size of two to four inches spread a min. of 4" thick to the limits shown on the plans.
3. The treated timber planks shall be #2 grade min., and should be free from large and loose knots.
4. The guidelines shown hereon are suggestions only and may be modified by the Engineer.

DATE:  
FILE:

		Design Division Standard	
<b>TEMPORARY EROSION, SEDIMENT AND WATER POLLUTION CONTROL MEASURES</b> <b>CONSTRUCTION EXITS</b> <b>EC(3)-93</b>			
FILE#	ec393.dgn	DWG	TxDOT
DATE	June 1993	CHKD	HEJ
REVISIONS		DESIGN	BD
		JOB	HIGHWAY
		DIST	COUNTY
			SHEET NO.